

Overton

Design guidance and codes

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Quality information

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Contents

1	1. Introduction	5
	1.1 Purpose of this report	5
	1.2 Process to prepare the design guide	6
	1.3 Area of study	7
	1.4 How to use this guide	9
	1.5 Planning policy and guidance	10
2	2. Neighbourhood Area Context Analysis	13
	2.1 Heritage assets	13
	2.2 Green and blue infrastructure	16
	2.3 Movement	20
	2.4 Land use	22
3	3. Design guidance and codes	24
	3.1 Introduction	24
	3.2 General design guidance and codes	26
	3.3 Checklist	66

Introduction

01

1. Introduction

Through the Ministry of Housing, Communities and Local Government (MHCLG) Programme led by Locality, AECOM was commissioned to provide design support to the Overton Parish.

1.1 Purpose of this report

This document seeks to set out how ‘good design’ process and guidelines can help to make future development as endearingly popular as the best of what has come before.

As the National Planning Policy Framework (NPPF) (paragraph 131) notes, ‘good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities’.

Research, such as for the Government’s Commission for Architecture and the Built Environment (now part of the Design Council; see, for example, The Value of Good Design¹) has shown that good design of buildings and places can improve health and well-being, increase civic pride and cultural activity, reduce crime and anti-social behaviour and reduce pollution.

To guide design on infill and speculative sites within the existing settlements and ensure any infill of new housing development fits in with existing patterns.



Figure 01: Barn conversion in Quidhampton.

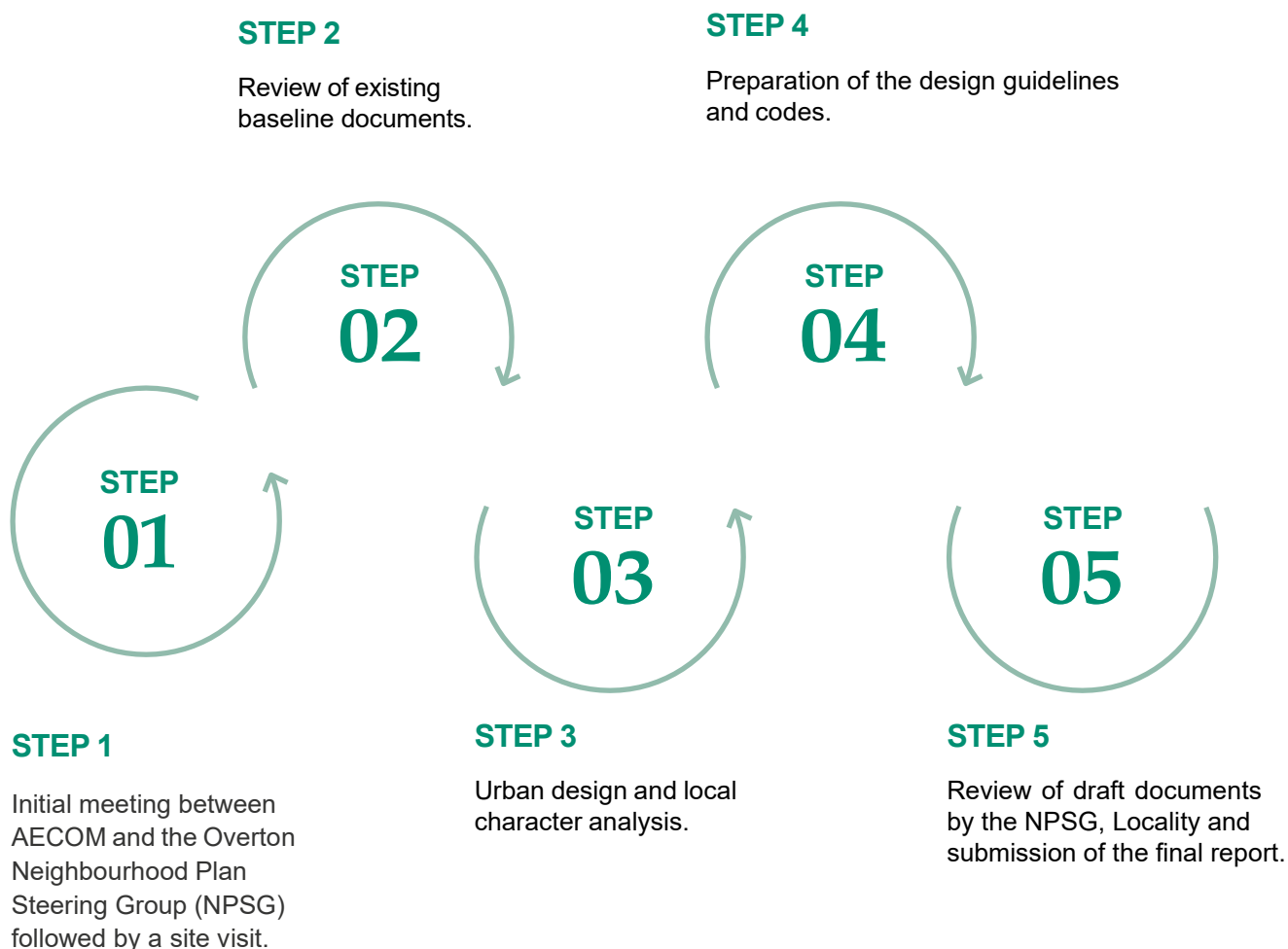
Figure 02: The River Test SSSI.

Figure 03: Old chapel building on Winchester Street.

1. <https://www.designcouncil.org.uk/sites/default/files/asset/document/the-value-of-good-design.pdf>

1.2 Process to prepare the design guide

The following steps were agreed with the Group to produce this report:



1.3 Area of study

Overton is a large village and civil parish located in Hampshire, west of Basingstoke and east of Andover and Whitchurch. The parish also contains the hamlets of Southington, Northington, Ashe, Polhampton and Quidhampton.

The settlement of Overton dates back centuries as is supported by its 47 listed buildings which remain intact today. 41 of these are located within the Conservation Area which, in tandem with the River Test, creates a rural and historic character to the village.

The parish is well connected by road and rail. The M3, A34 and A303 provide connections towards London, Southampton, Oxford and the south west of England. The railway station is located in Quidhampton, just to the north of Overton, providing regular services to London, Yeovil, Salisbury and Exeter. The village is also served by the 76 bus which stops regularly on the High Street and travels between Basingstoke, Andover and Whitchurch and the 86 bus which connects Basingstoke, Whitchurch and Winchester.

Overton is well served by a range of shops and facilities, which contribute significantly to community life in the area. In the neighbourhood area there are: 3 nurseries, a primary school, a 9 hole golf course, an 18 hole golf course, the 2 sports grounds, a surgery, 3 churches, and an electric mix of independent and chain retailers located in the village centre.



Figure 04: Overton village centre street scene.

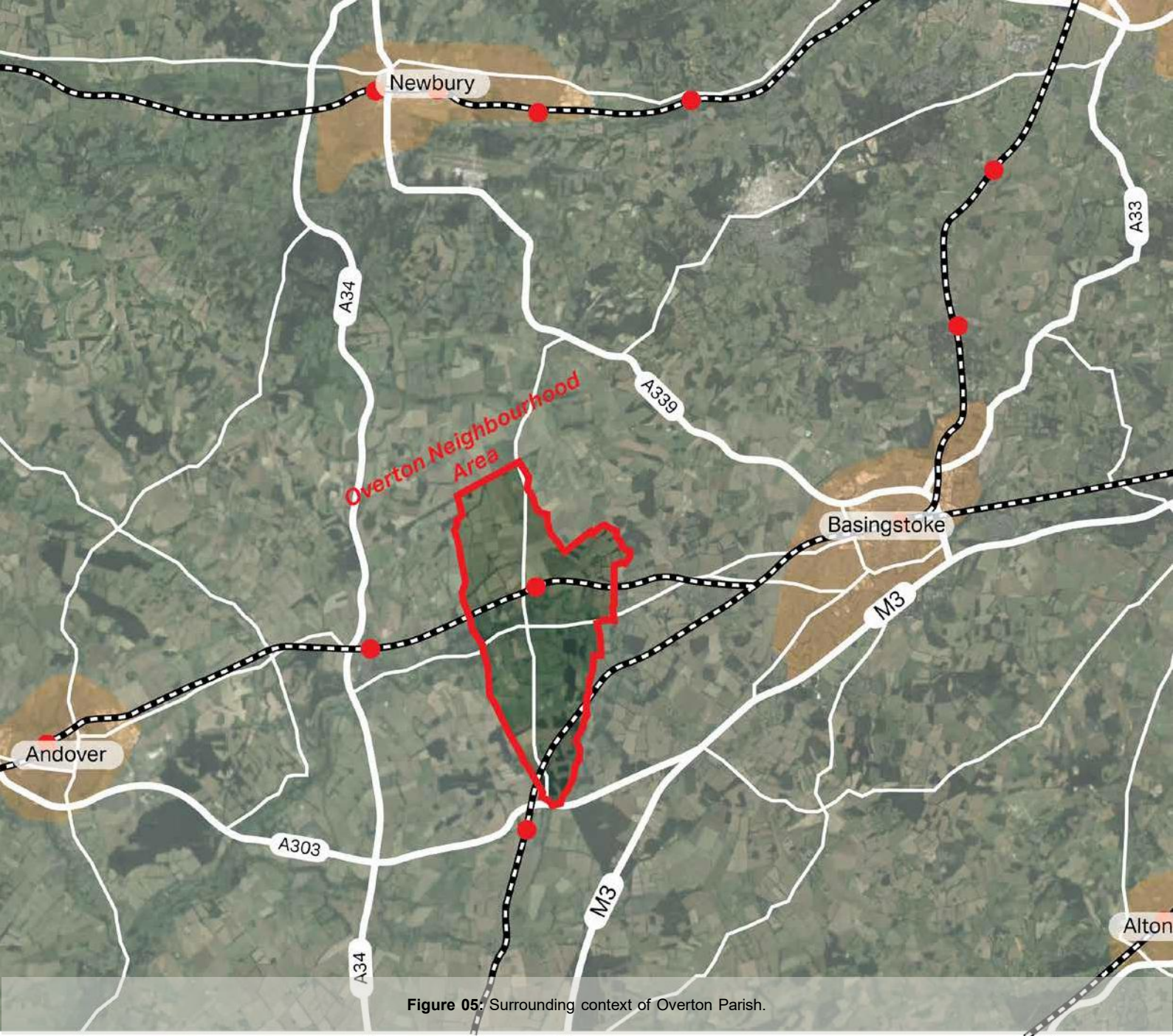


Figure 05: Surrounding context of Overton Parish.

KEY

-  Boundary
-  Roads
-  National Railway
-  Railway Station
-  Settlement



1.4 How to use this guide

The Design Guidelines & Codes will be a valuable tool in securing context-driven, high quality development in Overton Parish. They will be used in different ways by different actors in the planning and development process, as summarised in the table.

Actors	How they will use the design guidelines
Applicants, developers, & landowners	As a guide to community and Local Planning Authority expectations on design, allowing a degree of certainty – they will be expected to follow the Guidelines as planning consent is sought.
Local Planning Authority	As a reference point, embedded in policy, against which to assess planning applications. The Design Guidelines should be discussed with applicants during any pre-application discussions.
Parish Council	As a guide when commenting on planning applications, ensuring that the Design Guidelines are complied with.
Community organisations	As a tool to promote community-backed development and to inform comments on planning applications.
Statutory consultees	As a reference point when commenting on planning applications.

1.5 Planning policy and guidance

The following documents have informed this report. These guidelines have been produced at national and local level.

Any new development application should be familiar with these documents and make explicit reference to how each of them is taken into account in the design proposals.

2025 - National Planning Policy Framework

Ministry of Housing, Communities and Local Government (MHCLG)

Relevant national planning policy is contained within the National Planning Policy Framework. The NPPF was updated in December 2024 and includes reference to the National Design Guide and National Model Design Code and the use of area, neighbourhood and site-specific design guides. Paragraph 131 states that:

“the creation of high-quality buildings and places is fundamental to what the planning and development process should achieve and outlines that good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities”.

2021 - National Model Design Code MHCLG

This report provides detailed guidance on the production of design codes, guides and policies to promote successful design. It expands on 10 characteristics of good design set out in the National Design Guide.

2021 - National Design Guide MHCLG

The National Design Guide illustrates how well-designed places that are beautiful, enduring and successful can be achieved in practice. The 10 characteristics identified includes: context, identity, built form, movement, nature, public spaces, uses, homes and buildings, resources and lifespan. The Guide also reinforces the NPPF’s objective in creating high quality buildings and places. The document forms part of the government planning practice guidance.

2020 - Building for a Healthy Life Homes England

Building for a Healthy Life (BHL) is the new (2020) name for Building for Life, the government-endorsed industry standard for well-designed homes and neighbourhoods. The BHL toolkit sets out principles to help guide discussions on planning applications and to help local planning authorities to assess the quality of proposed developments, but can also provide useful prompts and questions for planning applicants.

2025 - Future Homes Standard

The Future Homes Standard will require new build homes to be future-proofed with low carbon heating and world-leading levels of energy efficiency; it will be introduced by 2025.

All homes will be ‘zero carbon ready’, becoming zero carbon homes over time as the electricity grid decarbonises, without the need for further costly retrofitting work.

From 2025, new homes built to the Future Homes Standard will have carbon dioxide emissions at least 75% lower than those built to current Building Regulations standards.

Local planning policy context

Local planning policy provides guidance that is tailored to the local context, where the development is located, which is supported by analysis taken directly from the area. Therefore, it is vital local policy is considered when proposing development within Overton Parish.

2016 - Basingstoke and Deane Adopted Local Plan

Basingstoke and Deane Borough Council

Basingstoke and Deane Borough Council adopted the Local Plan 2011 to 2029. The adopted Local Plan forms part of the statutory development plan for the borough. It sets out the council’s vision and strategy for the area until 2029 and will provide the basis for decisions on planning applications.

Draft Basingstoke and Deane Local Plan Update

Basingstoke and Deane Borough Council

The Basingstoke and Deane Borough Council is in the process of updating the Local Plan and is currently in consultation stage with the Local Plan anticipated to be adopted in 2025.

The Local Plan will have influence on the Overton Design codes and Neighbourhood plan. It is understood that the Overton Mill site has been included in the Local Plan as a strategic site for development of 340 homes.

Building with Nature - Standards Framework

Building with Nature

The UK’s first green infrastructure benchmark defines ‘what good looks like’, with a simple, easy-to-use framework that’s free to download.

The 12 Standards are built around the themes of Core, Wellbeing, Water, and Wildlife, helping residential and commercial developers to design and deliver high-quality green infrastructure, and guiding policy makers in clearly defining requirements for green-infrastructure in policy documents.



Neighbourhood Area
context analysis

02

2. Neighbourhood Area Context Analysis

This chapter describes the local context and key characteristics of the Parish related to history, landscape, movement and land use.

2.1 Heritage

Overton Parish has a rich history that dates back centuries. The area's name is believed to derive from the Old English words "ofre" meaning "riverbank" and "tun" meaning "settlement," indicating its location along the banks of a river.

In medieval times Overton was an agricultural village focused on farming and trade. Its position along the River Test proved economically significant, facilitating the transport and trade of goods. During the Middle Ages, Overton experienced growth and development, evidenced by the construction of St. Mary's Church, which is one of the 47 listed buildings within the parish. 41 of these listed buildings are located within the Conservation Area as displayed in figure 12.

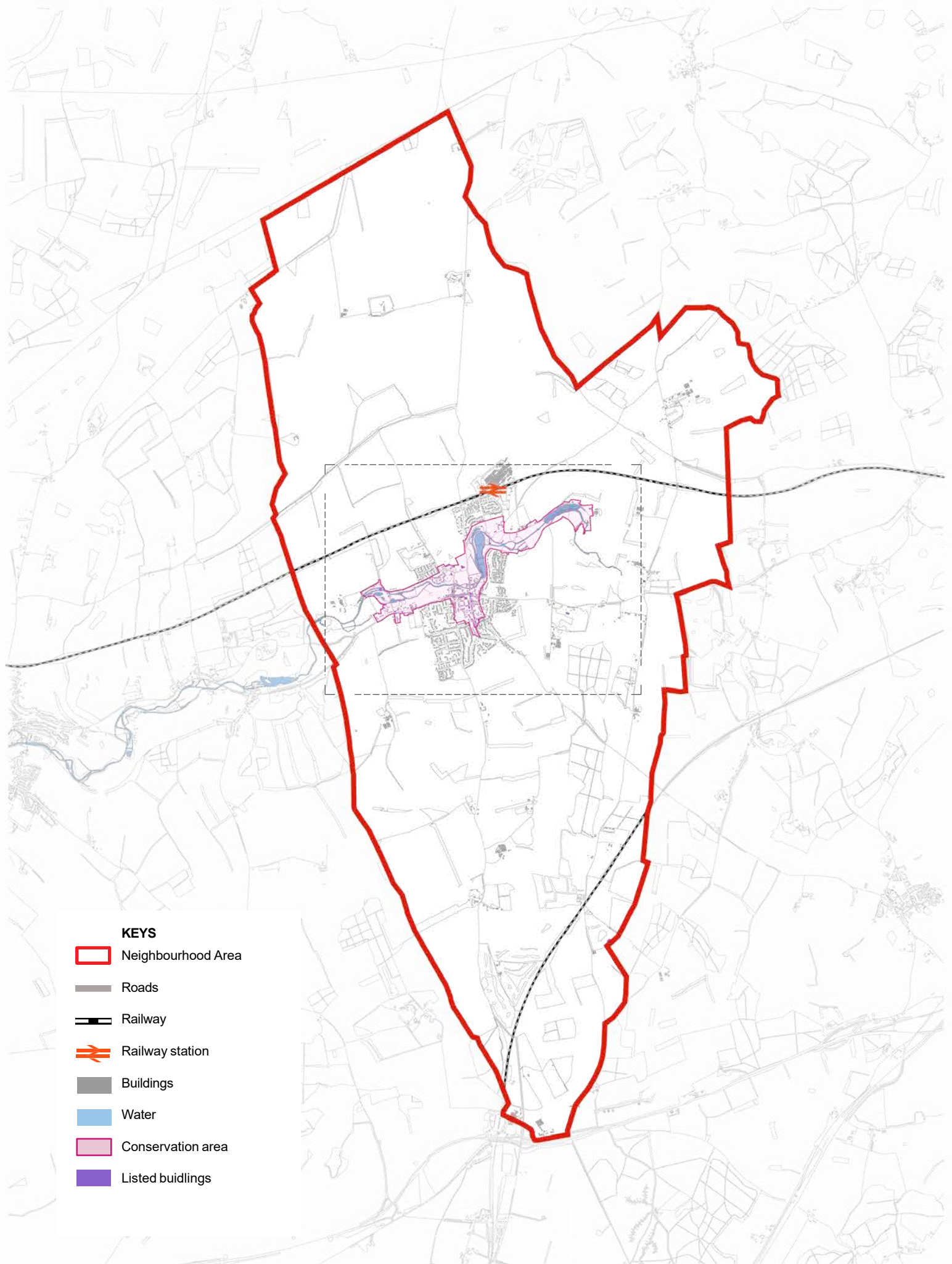
The arrival of the railway was primarily for the transport of goods originally, and the Mill was built next to the station to facilitate the movement of raw materials into the Mill itself using the siding that ran directly into the site as well as bringing workers from the neighbouring towns and villages to work there.



Figure 06: St Mary's Church.

Figure 07: Red Lion Lane cottages.

Figure 08: View down the High Street, within the Conservation Area.



KEYS


-  Neighbourhood Area
-  Roads
-  Railway
-  Railway station
-  Buildings
-  Water
-  Conservation area
-  Listed buildings

Figure 09: Overton historic assets map. OS ref: SU 51741 49744

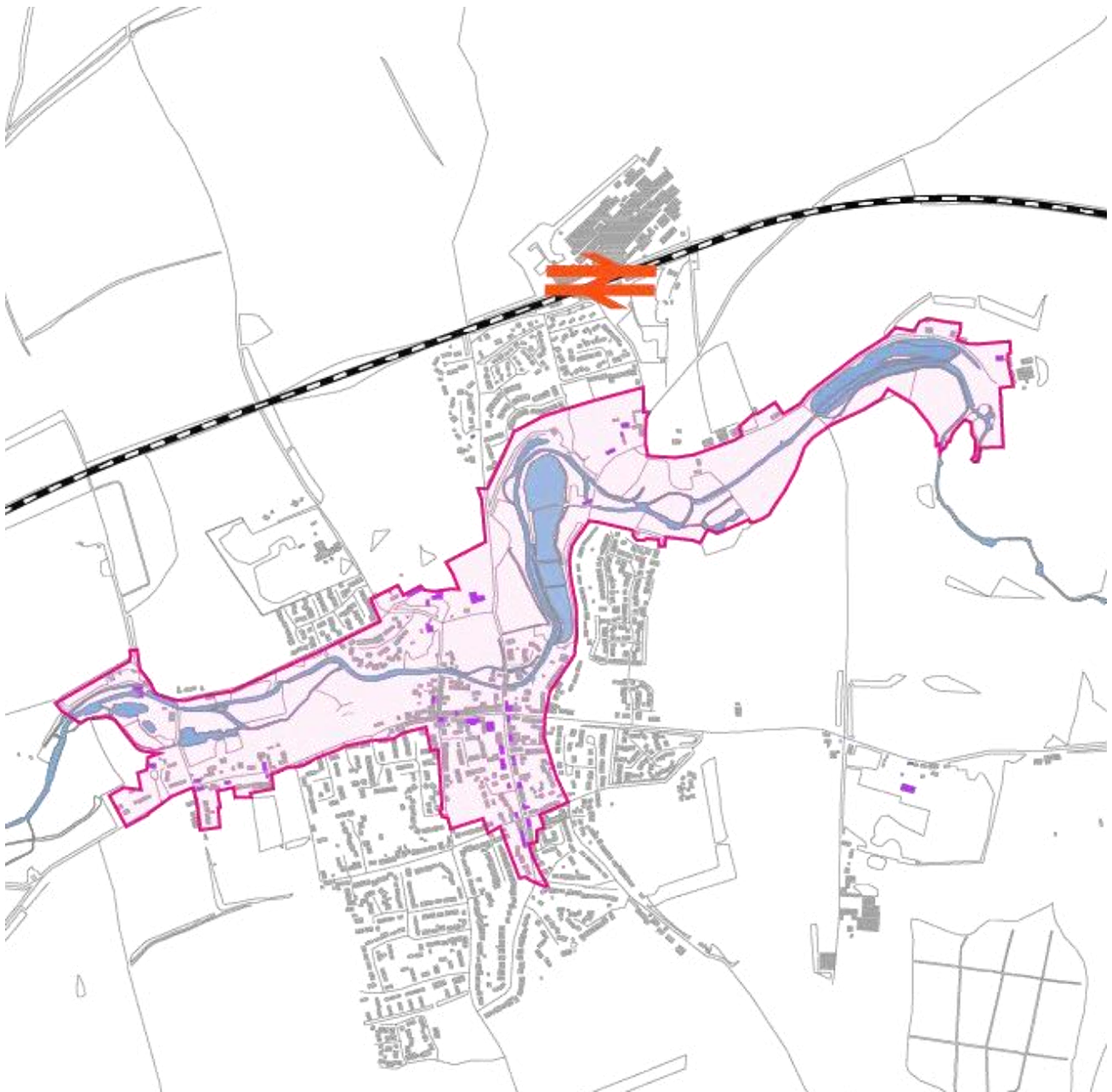


Figure 10: Overton historic assets map zoom in. OS ref: SU 51741 49744

- KEYS**
-  Roads
 -  Railway
 -  Railway station
 -  Buildings
 -  Water
 -  Conservation area
 -  Listed buildings



Figure 11: St Mary's Church.



Figure 12: Historic property on Winchester Street.

2.2 Green and blue infrastructure

In the village of Overton itself, are several open spaces, perhaps the most important of which is the survival of the wide main market street. There is a Sports Ground to the south of Church Road and one at Berrydown to the east of the village plus another 2 recreation grounds namely Edward Kersley and Sapley Lane.

There are also two golf courses within the neighbourhood area. Test Valley Golf is located in the south of the neighbourhood area. There is also Overton Town Meadow Golf Club which is a 9 hole course that runs alongside the River Test in the centre of the village.

Much of the farm land to the north of the main settlement falls within the North Wessex Downs National Landscape. In addition to the large arable fields within the National Landscape the village is also surrounded by a large amount of open fields with distinct hedgerows and woodlands. The rural views from the village to the surrounding hills and open areas are highly valued by the local community.

The River Test is a distinctive feature running through the Parish, flowing westward. As a result, part of the village and the surrounding land lies within the floodplain and is designated as Flood Risk Zone 3 by the Environment Agency. The River Test corridor extends from its source near Overton, through the southwest borough boundary near Whitchurch, and

continues south until it reaches the sea at the head of Southampton Water. The River Test, a nationally renowned chalk river, is designated as a Site of Special Scientific Interest (SSSI).”

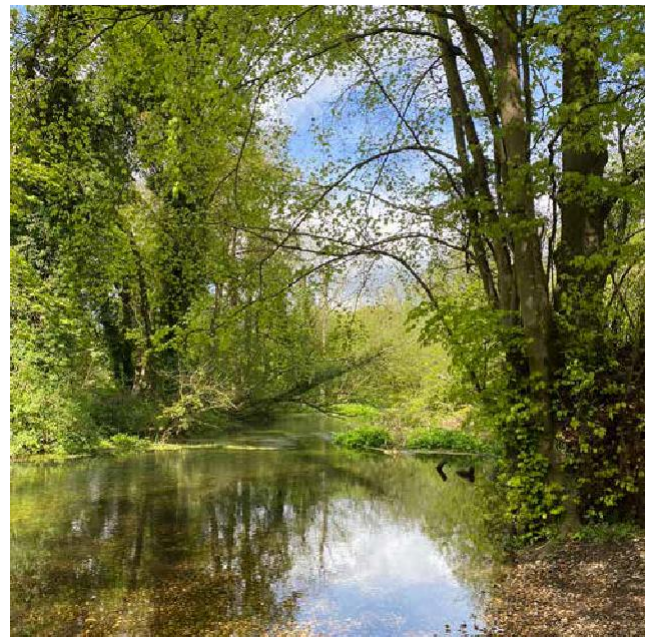


Figure 13: The River Test.



Figure 14: Overton sports ground.

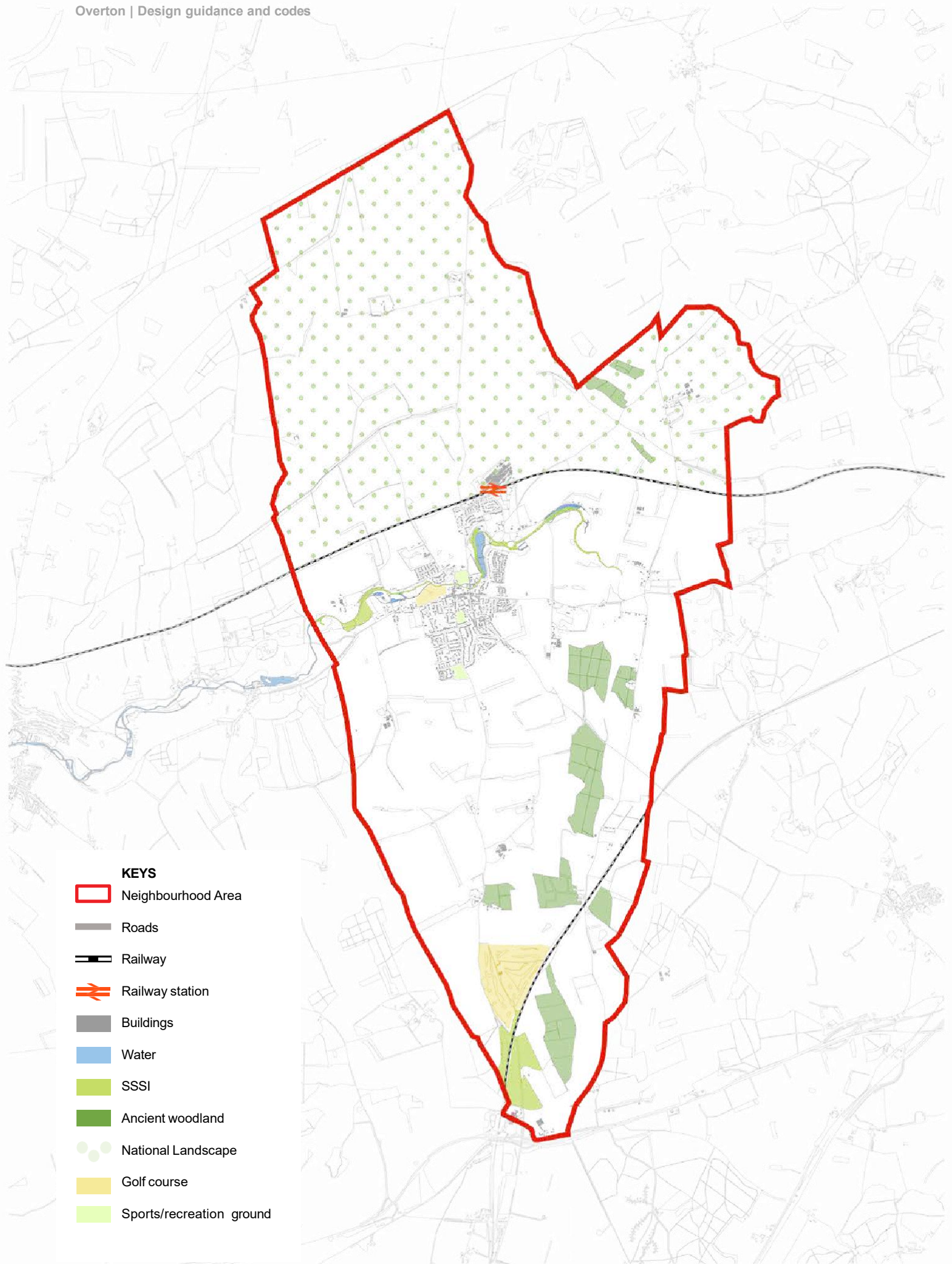


Figure 15: Overton green and blue infrastructure map. OS ref: SU 51741 49744



Figure 16: Overton green infrastructure map zoom in. OS ref: SU 51741 49744

- KEYS**
-  Neighbourhood Area
 -  Roads
 -  Railway
 -  Railway station
 -  Buildings
 -  Water
 -  SSSI
 -  Ancient woodland
 -  National Landscape
 -  Golf course
 -  Sports/recreation ground



Figure 17: Overton Town Meadow Gofl Club, from the High Street.



Figure 19: Overton flood risk map zoom in. OS ref: SU 51741 49744

- KEYS**
-  Neighbourhood Area
 -  Roads
 -  Railway
 -  Railway station
 -  Buildings
 -  Water
 -  Flood risk zone



Figure 20: River Test.



Figure 21: House that borders the River Test.

2.3 Movement

Overton is a very well-connected settlement to the benefit of all living or travelling there. The railway station, which is actually located in Quidhampton, provides regular services to London, Yeovil, Salisbury and Exeter. Also, just a few kilometres to the south of the village, on the edge of the parish, is the A303 and the M3. These as well as the B3400 provide direct access to the neighbouring towns, making Overton a popular place for commuters.

The High Street, Winchester Street and Kingsclere Road are the busiest routes in the village and meet at a cross roads in the centre of Overton. In terms of layout, the village is comprised of several larger blocks which are filled in with small residential cul-de-sacs.

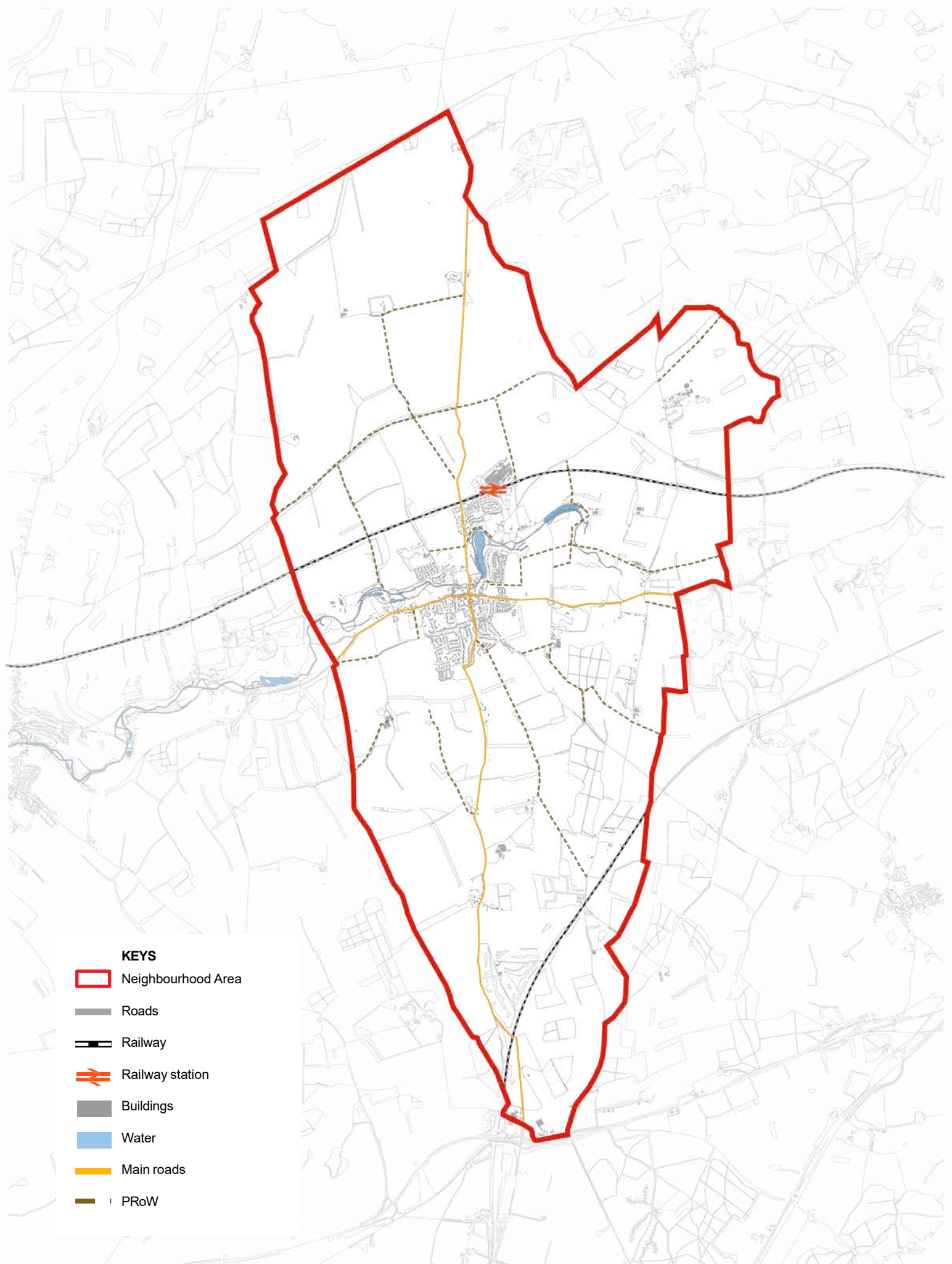
The community of Overton and Quidhampton benefit from many public footpaths that make the most of the green and blue assets in and around the settlement. The footpaths surrounding the River Test are fantastic for both winter and summer walks. The footpaths also allow for children to walk safely to and from school.



Figure 22: Overton Railway station (Quidhampton).



Figure 23: PRoW that runs along the River Test.



KEYS

-  Neighbourhood Area
-  Roads
-  Railway
-  Railway station
-  Buildings
-  Water
-  Main roads
-  PRoW

Figure 24: Overton movement map. OS ref: SU 51741 49744

2.4 Land use

The predominant land use in the parish is agricultural however it is residential in the built up areas. It has grown steadily over the years with various housing developments sprawling out from the historic core of Overton. Over the last Neighborhood Plan there have been 138 dwellings built in the Parish, these being the Seven Acres development west of Sapley Lane and the Sovereign Gate development off Two Gate Lane. A further 340 homes are anticipated to be delivered at the disused Overton Mill site.

Despite it being a rural village, Overton is very well served by shops and other employment opportunities. The main employment area of the village is concentrated within the Overton Village and comprises independent retailers and chain shops, pubs and restaurants, coffee shops, and offices. It was reported that there is a thriving local business network of professionals within Overton.

The main employment facility was the Overton Mill and associated offices which closed in 2022/23. The site has now been identified for a housing-led regeneration of the site and will be included in the emerging Basingstoke and Deane Local Plan.

Other facilities within the area include Overton Primary School, three nursery schools, Overton Station, a nine hole golf course, an eighteen hole golf course, Pharmacy, Overton Dental Surgery, Overton Surgery, the sports grounds and three active churches.

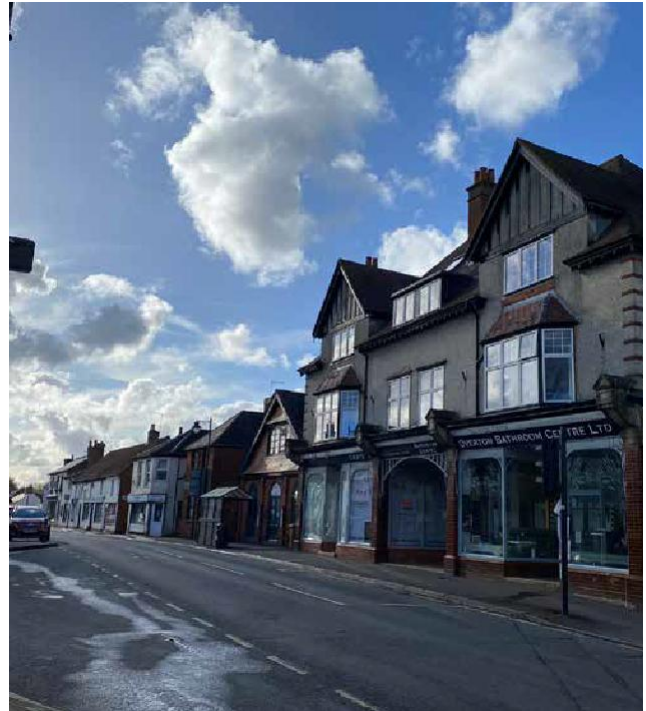


Figure 25: Shopping scene on the High Street.



Figure 26: The disused Overton Paper Mill site.

Design guidelines and codes

03



3. Design guidance & codes

This chapter provides guidance on the design of development, setting out the expectations that applicants for planning permission in the Parish will be expected to follow.

3.1 Introduction

The following section describes a set of design codes that have been put together based on the existing context of Overton Parish.

These codes will aim to guide any changes or development within the Parish to ensure the local character is respected whilst allowing space for innovation within the built environment.

The design codes have been split into four categories.

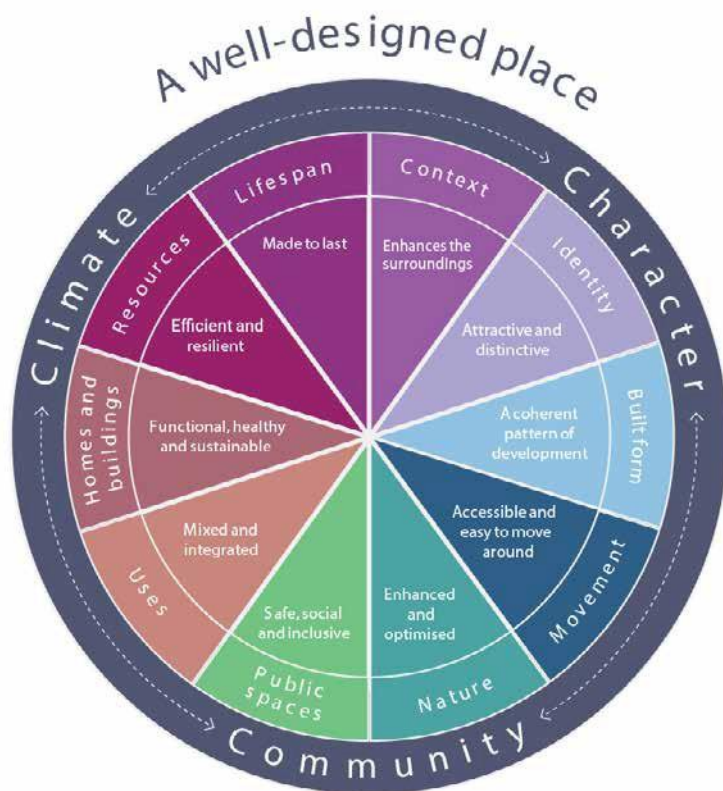


Figure 27: The 10 characteristics of well-designed places. (Source: National Design Guide, page 8).

3.1.1 Placemaking and design codes

These design guidelines and codes are underpinned by a set of placemaking principles that should influence the design of future development areas, public realms, homes and green spaces, and the interfaces between them.

What designers and planners call 'placemaking' is about creating the physical conditions that residents and users find attractive and safe, with good levels of social interaction and layouts that are easily understood.

The placemaking principles set out in the following pages should be used to assess the design quality of future development or regeneration proposals. These key principles should be considered in all cases of future development as they reflect positive placemaking and draw on the principles set out in many national urban design best practice documents including the National Design Guide, Building for a Healthy Life and the Urban Design Compendium.

The guidelines developed in this part focus on residential environments. However, new housing development should not be viewed in isolation, but considerations of design and layout must be informed by the wider context.

The local pattern of lanes and spaces, building traditions, materials and the natural environment should all help to determine the character and identity of a development.

It is important with any proposal that full account is taken of the local context and that the new design embodies the 'sense of place'.

Reference to context means using what is around, shown in the first two chapters, as inspiration and influence and it could be a contemporary solution that is in harmony with the surroundings.

3.1.2 Structure of the design guidance and codes

Based on the understanding gained in the previous chapters, this section will identify design codes for future development to adhere to. As identified in the diagnostic report, the following design guidance and codes have been created to apply to the whole Parish.

3.2 General design guidance and codes

Based on the understanding gained in the previous chapters, this section will identify design codes for future development to adhere to. The following design codes have been created to apply to the whole Parish.

Theme	Code	Title
Settlement Layout (SL)	SL 01	Patterns of development
	SL 02	Layout of buildings
Street, cycling and parking (SP)	SP 01	Active travel
	SP 02	Car parking solutions
	SP 03	Trees and landscaping on streets
	SP 04	Street lighting and dark skies
Built form (BF)	BF 01	Overlook public space
	BF 02	Accessible and attractive footpath network/access to the countryside
	BF 03	Define front and back gardens
	BF 04	Design workspace into new developments
	BF 05	Building line
	BF 06	Desired height profile
	BF 07	Respect locally important views
	BF 08	Extensions
	BF 09	Infill developments
	BF 10	Architectural details, materials and colour palette
Environmental and Energy Efficiency (EE)	EE 01	Features in dwellings
	EE 02	Sustainable Urban Drainage System
	EE 03	Rainwater harvesting
	EE 04	Permeable pavements
	EE 05	Waste storage and servicing
	EE 06	Wildlife friendly features
	EE 07	Electric vehicle charging points

SL. Settlement layout

SL 01 PATTERNS OF DEVELOPMENT

The Parish features a combination of linear development along Winchester Street and modern cul-de-sacs like Foxdown reflecting its blend of historical and contemporary layouts. New developments should respect this heritage by aligning with the traditional and modern architectural patterns evident in areas from High Street to Bridge Street. New developments should adhere to the following guidelines to respect the area’s heritage and structure:

- a) Development proposals should ensure continuity in the built environment along main routes, avoiding monotony by offering a diverse mix of building types and designs that are unified in scale, massing, and detail.
- b) Main road frontages should be enhanced with elements characteristic of the Parish, such as trees, hedgerows, wooden fences, red brick, or flint walls, to

strengthen the sense of enclosure and maintain the linear appearance.

- c) The linear settlement pattern typically faces inward toward the main road, with the rear turning away from the landscape. Where feasible, building frontages should emphasise this linearity.
- d) Boundary treatments should vary, ranging from low walls to soft landscaped edges at the settlement’s periphery, to avoid harsh transitions to the countryside.



Figure 28: Diagram showing a linear development pattern in the centre of Overton.



Figure 29: Pattern of development and active edges within a newly built estate block in Overton.

SL 02 LAYOUT OF BUILDINGS

The Parish’s distinctive charm stems from the historical development of its buildings and settlements. Overton features a notably compact but open settlement pattern. It is crucial for new developments to respect the unique architectural styles and patterns of each settlement to enhance their overall character. Specifically:

- a) New development should embrace the sense of enclosure found in all local communities, aiming to integrate seamlessly with the existing settlement structures by adopting similar features.
- b) Development planning should take a holistic approach, considering the impact and integration at the level of the entire settlement rather than planning in isolation.

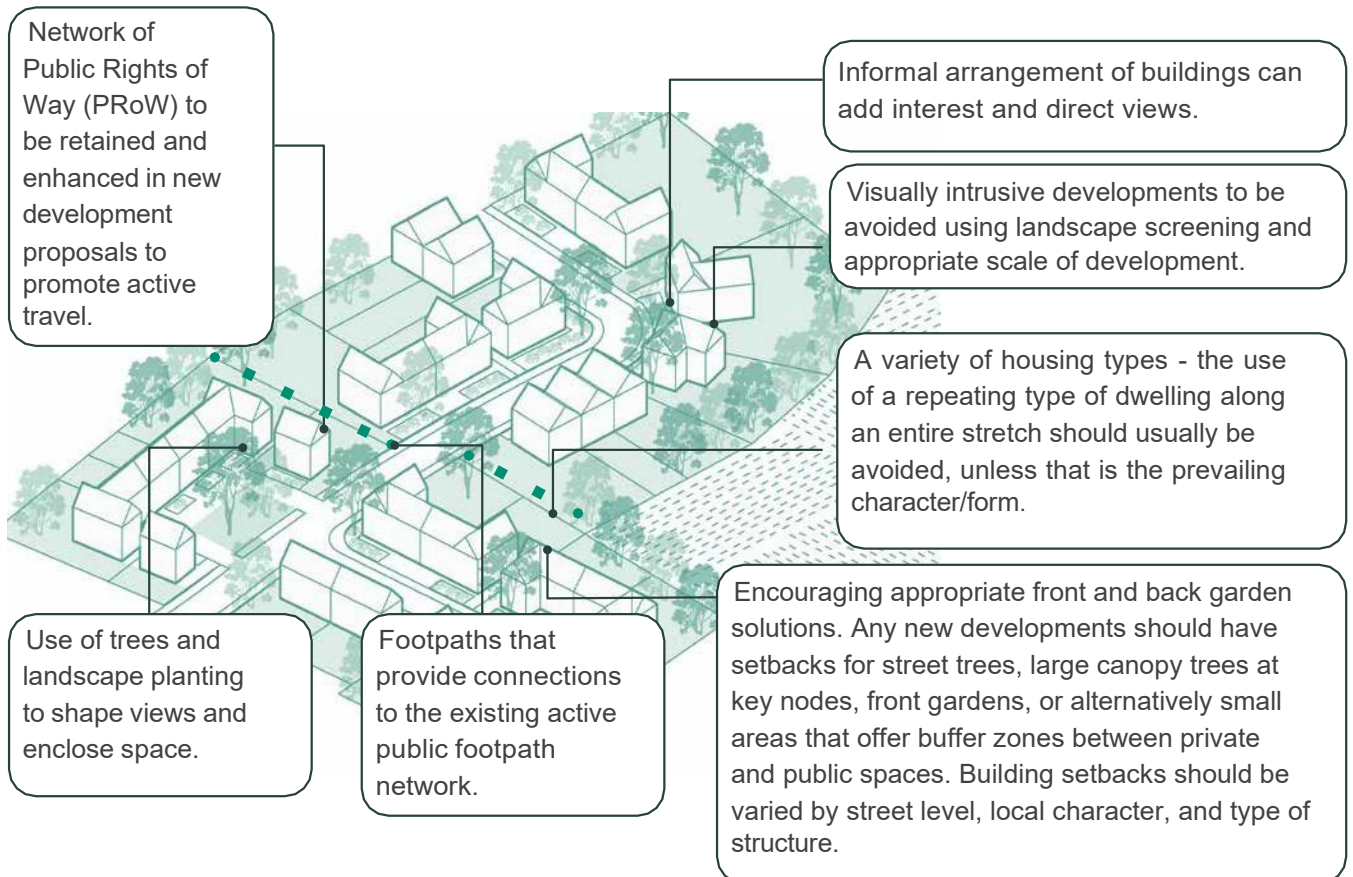


Figure 30: Diagram showing the layout of building elements such as enhancing PRoW networks, respecting views and front and back garden solution which could positively contribute to local character.

Development affecting the Conservation Areas and listed assets

The Parish is enriched with historically significant elements that contribute to its unique character, including the presence of Grade II and II* listed buildings among its settlements. Listed buildings such as St. Mary's Church, Court Farm with its medieval barn, and the diverse historic properties along High Street and Winchester Street, which showcase a mix of Georgian and Victorian architecture, should be sensitively maintained. To ensure new developments harmonise with these heritage assets, the following design guidelines have been established:

- a) Proposals for new buildings should avoid being visually obtrusive or obstructing important sightlines to and from heritage sites. This can be achieved by careful consideration of the buildings' scale and design, incorporating screening where necessary.
- b) New developments should preserve existing open spaces, vegetation, and trees, maintaining the historic layout and development patterns of the Parish.
- c) The size and bulk of new buildings should be carefully considered to ensure they are in harmony with nearby heritage assets.
- d) The preservation of gaps between buildings, as well as open views and vistas, is crucial. These spaces should be designed to highlight the importance of the heritage assets within the landscape.



Figure 31: Houses along the High Street.

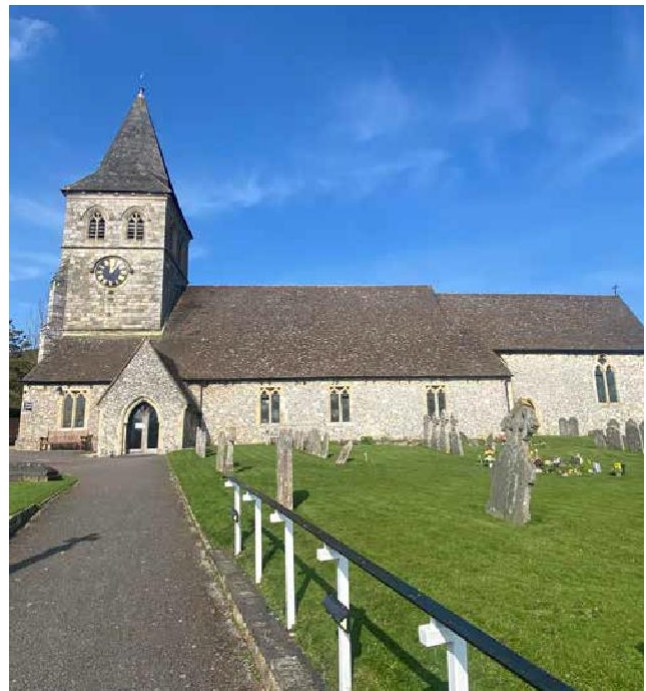


Figure 32: St Mary's Church.

SP. Street and Parking

The following pages set out policies to consider when developing both existing and new development within the Parish. They are generic design codes that apply to all areas of the Parish and are not specific to one area.

SP 01 ACTIVE TRAVEL

Enhancing the Parish’s walkability and cycling accessibility is key to improving residents’ health and their overall quality of life. The design guidance for pedestrian footpaths and cycle networks emphasizes the integration and improvement of key routes such as the paths through Flashetts, Brick Kiln Lane, Dellands and across North Field to the school.

- a) New developments should be strategically chosen to minimise car usage, encouraging active modes of transportation which are essential for creating ‘liveable’ neighbourhoods.
- b) It’s crucial that new developments include well-integrated pedestrian and cycling routes to promote active travel, ensuring these pathways connect to essential village services and existing networks, thus creating a comprehensive area for walking and cycling.
- c) Designs must accommodate a diverse range of users, including those with disabilities, parents/carers with strollers, and young children, ensuring accessibility and safety for all.
- d) Pedestrian safety should be enhanced at side roads through the use of level

crossings and vehicle junctions set back from crossings, with ample footpaths, grass verges, or wide pavements to prevent pedestrian-vehicle conflicts. Paths should be made of permeable materials to manage run-off effectively.

- e) Walking paths should also aim to link with the wider rural footpath and bridleway network, extending the range of accessible outdoor spaces.
- f) The design of new developments should move away from cul-de-sac patterns, which impede the flow of pedestrian and cycling traffic. If cul-de-sacs are proposed, they must include connections to footpaths to maintain open and accessible routes.
- g) New developments should prioritise the expansion and maintenance of these networks, ensuring safe, accessible, and well-connected paths. This includes clear signage, appropriate lighting,
- h) and the seamless integration of these paths into existing networks, facilitating easy access to local amenities and encouraging a shift towards more sustainable travel methods.



Figure 33: Public footpath connecting the built environment with the surrounding countryside.

CYCLING PARKING SOLUTIONS

Houses without garages

- i) Residential properties without an on- site garage should include secure and sheltered bicycle parking within the property boundaries.
- j) Cycle storage should be situated in an accessible and convenient spot.
- k) If the bicycle parking is part of the main house or a standalone shed, it must feature an entrance at least 900mm wide and be at least 2 metres deep.
- l) Enhancing the bicycle parking area with plants and small trees is recommended for aesthetic integration.
- m) Secure cycle storage should be included in the development of any flats.

Houses with garages

- n) The minimum garage size should be 7m x 3.3 m. 650 x 750mm of extra space is designated to cycle parking at the side.
- o) Where possible, cycle parking should be accessed from the front of the building either in a specially constructed enclosure or easily accessible garage;
- p) The design of any enclosure should integrate well with the surroundings; and
- q) The bicycle must be removed easily without having to move the vehicle.

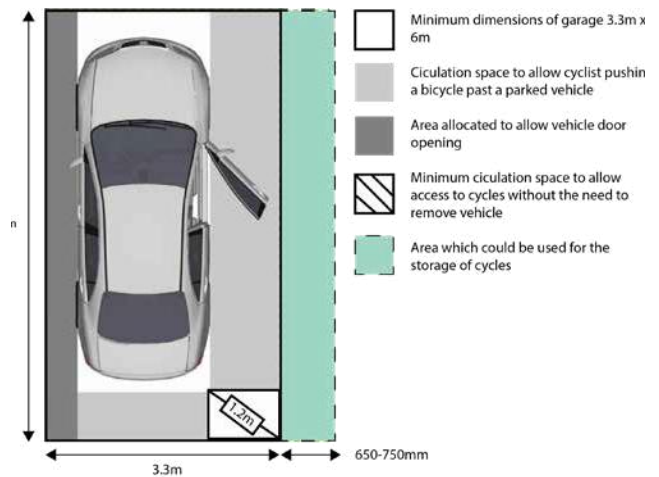


Figure 34: Minimum dimensions for garages including space for cycle storage (based on diagrams in Cambridge Cycle Parking Guide).

SP 02 CAR PARKING SOLUTIONS

Parking spaces are essential for modern living, but they should not detract from the appearance of a property or dominate the view. The design of parking areas should contribute positively to the environment, embodying the principles of placemaking.

- a) Parking at the front of a property requires careful design to reduce its visual impact and ensure it harmonises with the surrounding streetscape and materials. Strategies such as incorporating walls, hedging, planting, and high-quality paving can maintain a sense of privacy and interrupt the expanse of parking space. No more than 50% of the front garden should be dedicated to parking.
- b) Residential parking should offer a variety of configurations, including on-plot
- c) (side or front), garage, courtyard, and complemented by on-street options, to suit different needs and contexts.
- d) For family residences, parking should ideally be located to the side of the house or, if necessary, in front. In small housing developments, a rear courtyard for parking is also an acceptable solution.
- e) The design of parking spaces should integrate landscaping to visually soften the presence of vehicles.
- f) The construction of parking areas and driveways should focus on enhancing water permeability of surfaces to manage run-off effectively.



Figure 35: Generous on plot parking, elsewhere in the UK.



Figure 36: Current on-street car parking on Winchester Street.

On-Street Parking

On-street parking is widespread throughout the Parish, which detracts from the aesthetic appeal of street scenes and, when cars park on pavements, hinders the creation of pedestrian-friendly streets. To mitigate the visual disruption caused by parked cars, reliance on street parking should be minimised in future developments whenever feasible.

- g) On-street parking arrangements should be carefully planned to ensure they do not obstruct pedestrian, cyclist, and vehicular movement, while also potentially acting as informal traffic calming measures.
- h) To avoid long stretches of uninterrupted parked cars, no more than four vehicles should be parked consecutively without a street tree planted in between.
- i) In areas with low traffic where streets are shared by vehicles and pedestrians, parking spaces should be delineated with different paving materials rather than traditional road markings, enhancing visual clarity and safety.
- j) There should be a concerted effort to accommodate electric vehicle charging points in new public parking areas, taking advantage of the shift
- k) towards electric vehicles. Incorporating charging infrastructure into the
- l) design of roads and street furniture is essential for supporting this transition in both public and private spaces.

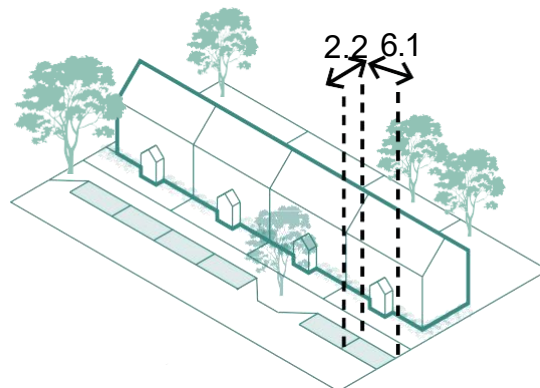


Figure 37: Positive example of on-street car parking from the Distinctively Local Report: <http://distinctively-local.co.uk/storage/app/media/case-studies.pdf>

Figure 38: Diagram illustrating optimal parking dimensions with regular crossing points to avoid cars becoming a barrier.

On-Plot Side or Front Parking

- n) Parking on driveways at the front of houses should be limited due to the visual clutter created by parked cars along the street. To minimise this impact, only two consecutive homes will be allowed to offer front driveway
- o) parking. Front gardens providing parking must be be at least 6 metres deep to accommodate vehicle movement and must include hedgerows for screening.
- p) Driveways beside homes must be long enough (at least 5 metres) to ensure parked cars do not extend beyond the front line of the house, thus lessening their visual presence from the street. A minimum front garden depth of 3 metres is required in these instances. Additionally, driveways and forecourts should utilise permeable materials to support effective water management.

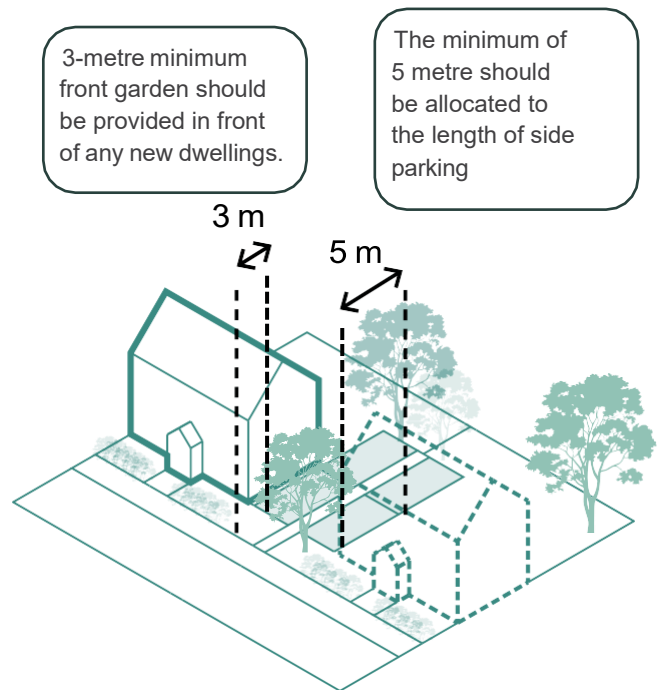


Figure 39: Illustrative diagram showing an indicative layout of on-plot side parking

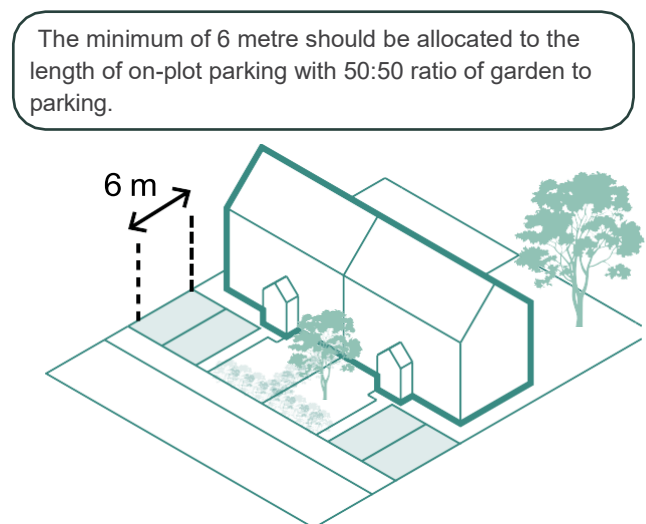


Figure 40: Illustrative diagram showing an indicative layout of on-plot front parking



Figure 41: Example of side of plot parking in the parish.



Figure 42: Front of plot parking on Winchester Street.

Garage Parking

Parking being provided in a garage to the side of a dwelling should be in line with or slightly set back from the frontage line of the existing dwelling, which is in keeping with the character of the existing Parish and will reduce the visual impact of cars on the street. Garages must be configured to ensure cycles can be easily accessed. Garages should also provide sufficient room for cars to park inside them as well as providing some room for storage.



Figure 44: Garage to the side of a property, on the River Test.



Figure 43: Illustrative diagram showing an indicative layout of on-plot garage parking.

Parking courtyard

- q) This parking layout is versatile and particularly well-suited for terraced houses along busy roads where direct access to individual parking spots is not feasible.
- r) Designated parking courtyards should be established.
- s) These parking areas should be positioned to benefit from natural oversight to enhance security.
- t) It's crucial for parking courtyards to be thoughtfully designed to integrate well with the surrounding public spaces. This means planning them with ample space for vehicles, utilising high-quality materials for both the pavement and landscaping elements.
- u) Parking spaces should be organised into small clusters, with no more than four spaces each. To enhance the environment, these clusters should be spaced out with trees and other greenery. This not only adds aesthetic appeal but also provides shade, mitigates the heat island effect, and reduces the amount of non-permeable surface area.

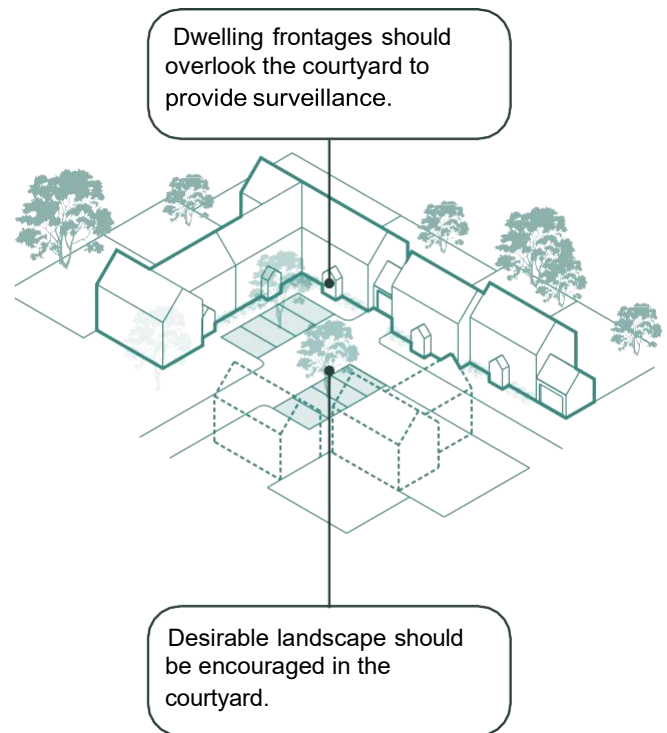


Figure 45: Illustrative diagram showing an indicative layout of parking courtyards.

SP 03 SAFEGUARD TREES AND LANDSCAPING ON STREETS

The Parish greatly benefits from its rich tree coverage, which enhances the environment and community well-being in numerous ways. Trees contribute to shading and cooling, carbon dioxide absorption, and air pollution reduction. They also provide vital habitats and ecological corridors for various species, aiding in water management and humidity control.

However, it has been observed that previous developments have not adequately integrated space for street trees into their residential estate designs. Beyond their environmental benefits, trees play a crucial role in improving mental health by reducing stress and anxiety, aiding recovery from illness, and fostering a sense of well-being. They also enrich the landscape, contributing to the character and appeal of open spaces.

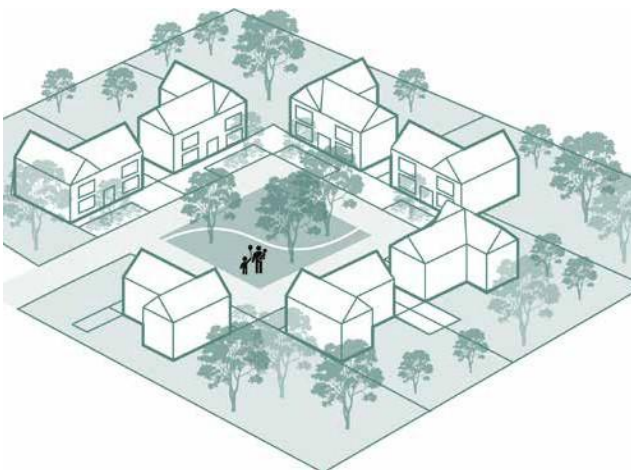


Figure 46: An indicative diagram showing green spaces and landscape planting.

The following guidelines are aimed at improving the planning and visual integration of trees and plants within both private gardens and public spaces, including streets, to maximise these benefits.

Planting standard

- a) Existing mature trees should be preserved, incorporating them into the new landscape design and using them as accents and landmarks, where appropriate;
- b) Consider canopy size when locating trees; reducing the overall number of trees but increasing the size of trees is likely to have the greatest positive long-term impact. Space should be created to establish large canopy trees, complementing street trees to provide a network of green infrastructure;
- c) Size of tree pits should allow sufficient soil around the tree and should be designed to provide relative m² for tree species using Root Zone calculator. Ensure tree stems are in the centre of the verge to provide a 1m clearance of the footway or carriageway;
- d) Tree root zones should be protected to ensure that trees can grow to their mature size. Root barriers must be installed where there is a risk of damaging foundations, walls and underground utilities;
- e) New trees should be added to strengthen vistas, focal points and movement corridors, while retaining clear visibility into and out of amenity spaces.

- h) New trees should be integrated into the design of new developments from the outset rather than left as an afterthought to avoid conflicts with above- and below-ground utilities;
- i) To ensure resilience and increase visual interest, a variety of tree species is preferred over a single one. Tree species should be chosen to reflect the prevailing character of the landscape, soil conditions and the associated
- j) mix of native species, but should
- k) also have regard to climate change, environmental/habitat benefits, size at maturity and ornamental qualities;
- l) Fruit trees are encouraged in the private gardens of new developments;
- m) Regulations, standards, and guidelines relevant to the planting and maintenance of trees are listed below:
- n) Trees in Hard Landscapes: A Guide for Delivery;¹
- o) Trees in the Townscape: A Guide for Decision Makers;²
- p) Tree Species Selection for Green Infrastructure;³ and

¹ Trees & Design Action Group (2012). *Trees in Hard Landscapes: A Guide for Delivery*. Available at: http://www.tdag.org.uk/uploads/4/2/8/0/4280686/tdag_trees-in-hard-landscapes_september_2014_colour.pdf

² Trees & Design Action Group (2012). *Trees in the Townscape: A Guide for Decision Makers*. Available at: http://www.tdag.org.uk/uploads/4/2/8/0/4280686/tdag_treesinthetownscape.pdf

³ Trees & Design Action Group (2019). *Tree Species Selection for Green Infrastructure*. Available at: http://www.tdag.org.uk/uploads/4/2/8/0/4280686/tdag_treespeciesguidev1.3.pdf

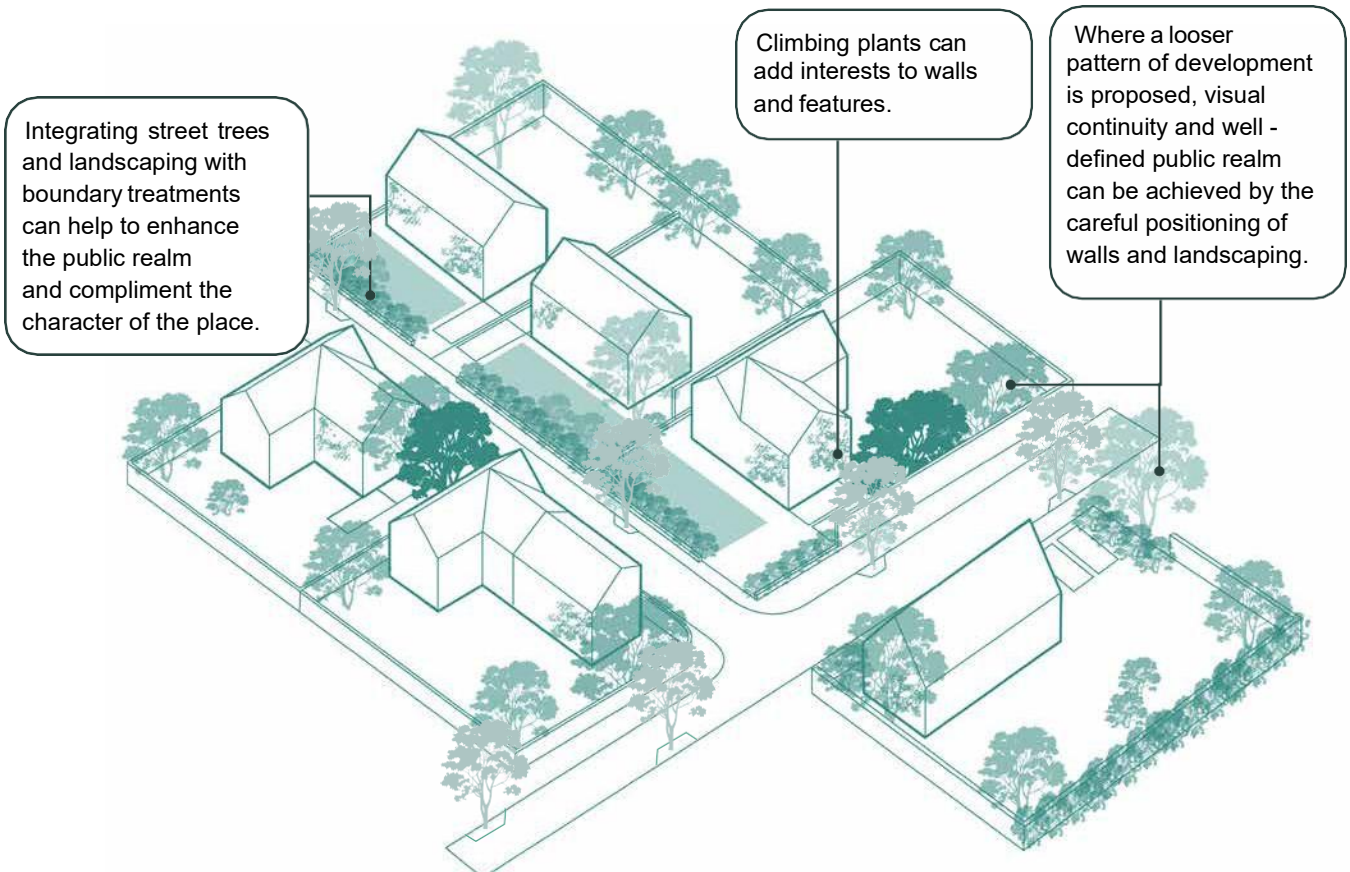


Figure 47: Diagram showing trees and landscaping that complement the public realm and create a sense of enclosure.

- q) BS 8545:2014 Trees: from nursery to independence in the landscape - Recommendations.¹

Give spatial enclosure, provide screening and privacy

Hedges, hedgerow trees, and walls play a crucial role in defining the area's distinctive character and fostering a sense of privacy and enclosure. To align with the area's existing aesthetic, both the architecture of buildings and the design of boundary treatments should reflect the prevailing style, though some variation is encouraged to enhance visual interest.

- r) It's important to preserve existing hedges, trees, and walls whenever possible, as they contribute significantly to the area's sense
- s) of enclosure. New or replacement plantings should be considered to ensure the continuous presence of hedge and hedgerow tree coverage.
- t) In new developments, where practical, buildings should be set back from the property line to allow for front gardens or, alternatively, a small planted area that serves as a buffer between private and public spaces, enhancing the sense of separation and privacy.

Complement public realm and enhance built environment and local identity

Strategic planting significantly improves the visual appeal of an area and contributes to its unique identity.

- u) New developments should incorporate boundary elements that harmonise with the street and enrich the Parish's character. Trees, hedges, and other plantings in areas visible to the public, such as edges and boundary lines, are highly recommended.
- v) Climbing plants are especially effective for concealing less attractive structures like garages, bare walls, and fences, adding greenery and aesthetic value.

Form focal points and frame views

In addition to the intrinsic value of trees, they can also have a practical use value. In a small-scale open space, trees provide a focal point of interest.

¹ British Standards Institution (2014). *BS 8545:2014 Trees: from nursery to independence in the landscape - Recommendations*. Available at: <https://shop.bsigroup.com/ProductDetail/?pid=000000000030219672>

SP 04 STREET LIGHTING AND DARK SKIES

Artificial lighting offers significant benefits, making spaces feel more inviting at night and enhancing safety for pedestrians and cyclists. However, in areas like Overton lighting must be carefully managed to prevent light pollution, preserving the natural dark skies that are advantageous for both wildlife and people.

New developments should strive to reduce their impact on these 'dark skies' and limit light pollution that can disrupt both natural ecosystems and human health. To this end, the following design guidelines are proposed:

- a) Encourage the integration of natural lighting in the design of new
- b) developments and house extensions.
- c) To protect bat populations, it is advised to use LED lamps with a colour temperature below 2700k, rather than mercury or metal halide lamps. The latter emit UV light that can alter insect distribution and attract bats, thus interfering with their natural behaviours (Bat Conservation 2008). Lighting near bat roosts within new developments should be avoided entirely.
- d) Implement a strategic lighting plan that
- e) ranks different light sources—such as lamp columns, bollards, Passive Infrared (PIR) sensors, porch lights,
- f) solar-powered lights, and various forms of path and accent lighting—according to their function. This hierarchy will guide the setting of light levels and timing for turning lights off.

- g) All lighting should have a colour temperature of less than 2700K to control light spill and glare effectively. Light shields can also be employed to mitigate glare and protect dark skies further.
- h) Select lighting options that are both energy-efficient and sustainable, with a preference for motion sensors that activate lighting only when needed, directing light carefully to where it is necessary.

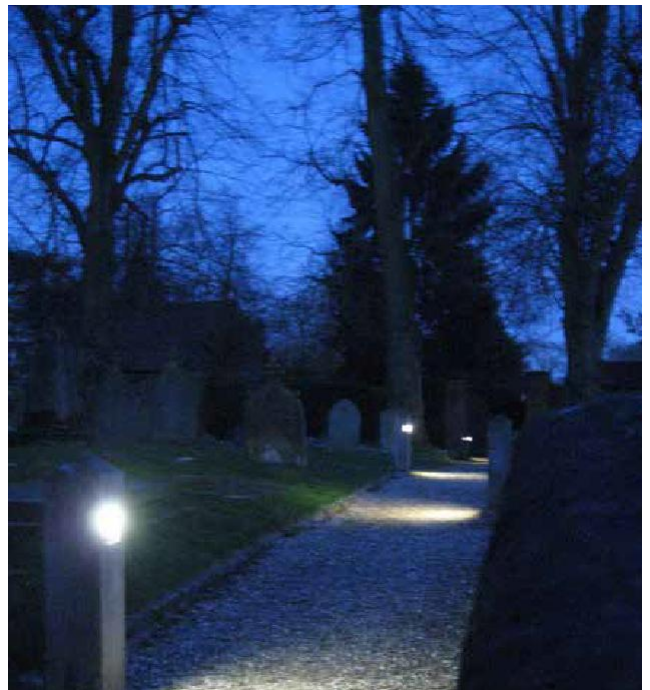


Figure 48: Example of a low level lighting solution at Lapworth churchyard. Photo by Robin Stott.

BF. Built Form

This section provides guidelines for developers planning new projects within the Parish, covering both new constructions and modifications to existing properties. It emphasises the need for thoughtful consideration in the development process, applicable to both new and current developments.

Typically, infill projects within the settlements feature large plots with ample outdoor spaces. This approach suits redevelopment in these areas well. However, for emerging areas, the development should follow a unified approach, aligning with contemporary best practices. This includes maintaining a balance in the plot sizes, the size of the buildings, and the spacing between them. Generally, Overton is characterised by its low-density development, with buildings averaging two storeys in height and positioned a comfortable distance apart. Illustrative diagrams are provided to convey these standards, and new proposals are expected to demonstrate adherence to these principles.

The guidelines are organised to address broader policy issues first, gradually narrowing down to specific details regarding the construction and design of the buildings.



Figure 49: Example of a typical 2 storey property with a steeply pitched porch in Overton.



Figure 50: Street scene with varying building heights within the Conservation Area.

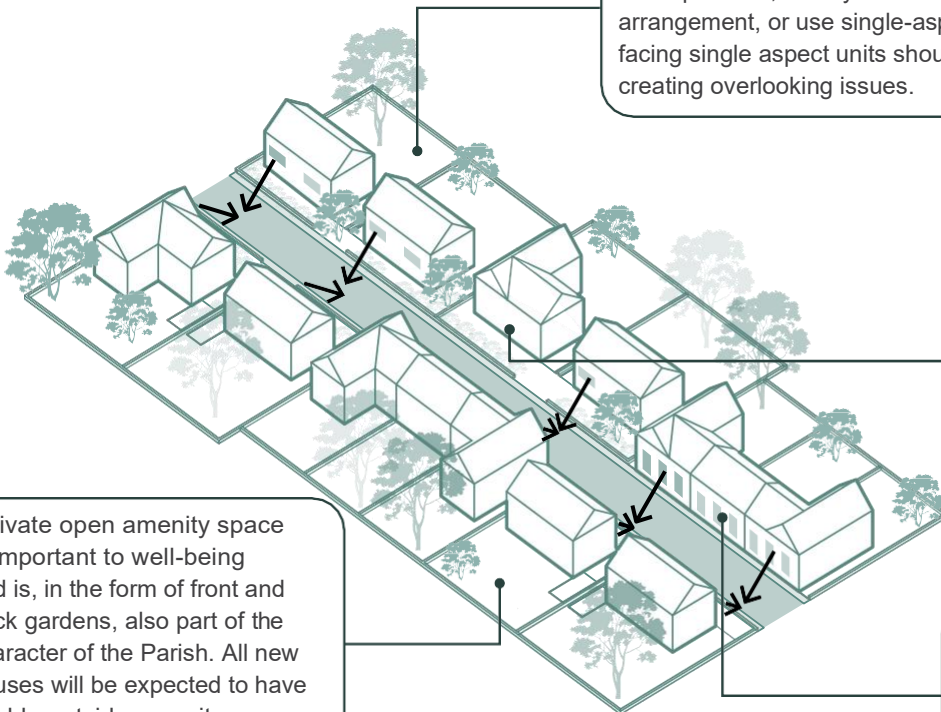
BF 01- OVERLOOK PUBLIC SPACE

To enhance security and enable natural observation, homes should be designed so that their windowed fronts face the street, aligning with the local area’s character. Placing the rear boundaries of properties directly against the street is discouraged, as it detracts from the street’s appearance, and diminishes security and surveillance opportunities. Instead, rear boundaries should either align with the backs of other properties or blend smoothly into natural surroundings, especially at the edges of settlements. Both front and back gardens should serve as green spaces, contributing to the increase of wildlife and biodiversity.



Figure 52: Recently converted residential buildings overlooking green space.

The privacy distance between the backs of the properties should be a minimum of 20m. Where this is not possible, the layout should be a back to-side arrangement, or use single-aspect buildings (north facing single aspect units should be avoided) to avoid creating overlooking issues.



Private open amenity space is important to well-being and is, in the form of front and back gardens, also part of the character of the Parish. All new houses will be expected to have usable outside amenity space.

Avoid inactive and blank façades which reduce the sense of security in public realm.

Windowed front elevations to be encouraged in order to improve natural surveillance.

Figure 51: Diagram to highlight the importance of natural surveillance to improve security and sense of safety.

BF 02- ACCESSIBLE AND ATTRACTIVE FOOTPATH NETWORK/ ACCESS TO THE COUNTRYSIDE

The Parish boasts several footpaths that connect villages with the surrounding countryside, offering picturesque walks that bring people closer to nature, provide a peaceful environment, and encourage walking as a form of exercise. In light of this, the creation, enhancement, and preservation of footpaths are vital considerations in the planning of new developments. Some design principles to guide this process include:

- a) Ensuring new developments maintain or introduce well-connected and appealing footpaths between neighbouring streets and local amenities. A strong pedestrian network that integrates new with existing areas is crucial for fostering connectivity and supporting walking and cycling.
- b) Proposed footpaths should link green areas and woodlands, forming a network of eco-friendly walking paths that enhance biodiversity.
- c) To help pedestrians and cyclists navigate and to raise awareness of accessible routes throughout and beyond the Parish, strategically positioned signage is essential. However, any new signs must be designed to complement the rural setting of the Parish without adding to visual clutter.



Figure 53: Appropriate material change to indicate the footpath/ cycle lane within a rural landscape, elsewhere in UK.



Figure 54: Bridleway that runs alongside the River Test.

BF 03- DEFINE FRONT AND BACK GARDENS

Maintaining a balanced ratio of garden space to the built structure on a plot is crucial for preserving the Parish's open and green character.

Garden sizes vary across different areas of the Parish. For example, in the historic core of Overton, front gardens typically measure between 0 to 5 metres in depth, while back gardens range from 5 to 15 metres. Elsewhere in the more rural parts of the parish properties have a much greater setback.

To ensure adequate outdoor amenity space, back gardens should have a minimum depth of 10 metres and offer a minimum of at least 50 square metres of usable area. This useable area would need to be increased to 60 sqm for houses of 3-bedrooms or larger or if required to enable the new plot to be in keeping with the character of the area. For gardens facing north, the length should exceed 10 metres to maximise sunlight exposure.

Where practical, front gardens should have a minimum depth of 3 metres.

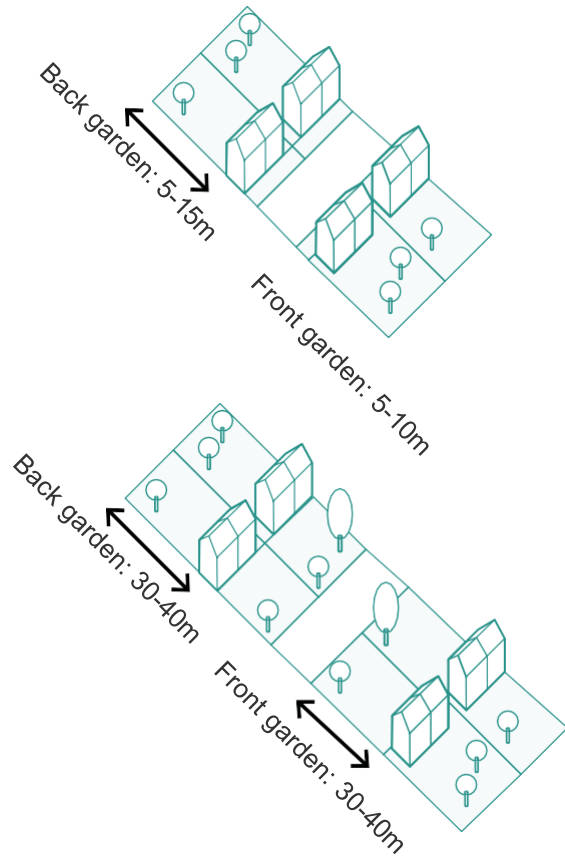


Figure 55: Diagram illustrating the existing variation in garden sizes within the Parish.

BF 05 - BUILDING LINE

The architectural arrangement of buildings, specifically the continuity of building lines and the distance they are set back from the street, plays a pivotal role in defining the area's character and the sense of space within streets and public areas. A uniform building line with minimal gaps effectively delineates public from private spaces and enhances the definition of the public domain. In cases where buildings are set further back from the road, these threshold areas should be attractively landscaped.

- a) To achieve a sense of enclosure along the street while maintaining private space, front thresholds should be designed with a modest depth,
- b) incorporating a small garden or planting area.
- c) Residential areas with low to medium densities can adjust their setbacks to align with the landscape's context and preserve the area's open feel.
- d) Front gardens may be designed deeper in response to specific topographical needs or to match the existing village character, facilitating a smoother transition from the countryside and green spaces to the built environment.



Figure 56: Uniform building line with no setback on Red Lion Lane.

BF 06 - DESIRED HEIGHT PROFILE

- a) Development building heights should accord with the settlement character of 1, 2 and where appropriate 3 storey;
- b) Roofs in the village tend to be generally traditionally pitched, with some hipped examples. New roof types and pitch should reflect this. Materials should be subservient to what is already in use throughout the Parish, however where solar panels are in use dark tiles should be the choice as they do not clash in terms of colour;
- c) Innovation which explores the integration of green roof should be encouraged;
- d) The scale of the roof should always be in proportion to the dimensions of the building itself. Flat roofs for buildings, extensions, garages and dormer windows should be avoided; and
- e) Chimney type and height should be congruent with the typical Parish chimney precedent examples.



Figure 57: Historic red brick terraces on Kingsclere Road.



Figure 58: Listed thatched cottage using a range of traditional materials.

BF 07 - RESPECT LOCALLY IMPORTANT VIEWS

It is important to craft short-distance views that are interspersed with buildings, prominent trees, or landmarks like St Mary's Church, enhancing the character of walking routes. Concurrently, preserving long-distance views across the River Test valley or towards the North Wessex Downs ensures pleasant sceneries along footpaths and roads, strengthening the visual connectivity across the village and promoting walking and cycling.

New developments should be strategically oriented to capitalise on these short and long-distance views, enriching the residential experience and community connection. Moreover, construction should avoid ridge tops, upper valley slopes, and other prominent positions that might obstruct these key vistas. Planning decisions must prioritise maintaining, or ideally enhancing, these important views and vistas, thus integrating new development seamlessly into Overton's unique landscape.



Figure 59: Example of a locally important view northwards from Winchester Street towards the Church spire and the field behind.

BF 08 - EXTENSIONS

There are a number of principles that residential extensions and conversions should follow to maintain character:

- a) Many household extensions are covered by permitted development rights and therefore do not need planning permission;
- b) Roof extensions should not take away from the character of the pitched roofs;
- c) The original building should remain the dominant element of the property regardless of the scale or number of extensions. The newly built extension should not overwhelm the building from any given viewpoint; and
- d) Extensions should not result in a significant loss to the private amenity area of the dwelling;

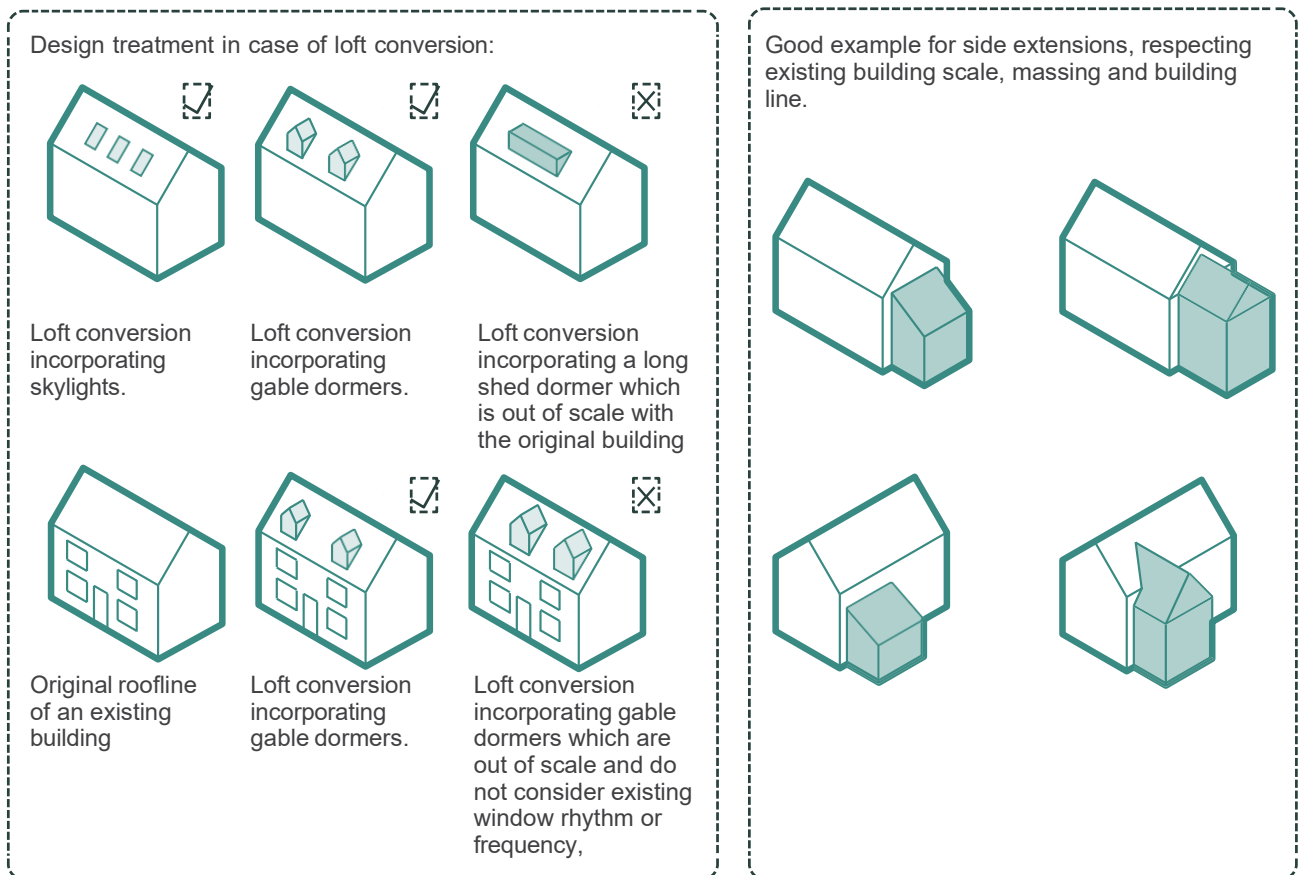


Figure 60: Some examples for different type of building extensions

BF 09 - INFILL DEVELOPMENTS

Infill sites will vary in scale, context and location within a settlement. Any new infill can have significant impact on the character and appearance of the built environment. The following principles should be applied in any future infill site:

- a) Infill development should complement the street scene into which it will be inserted. It does not need to mimic the existing styles but its scale, massing and layout need to be in general conformity with the existing (this is particularly ridge/eave heights, especially for terraced or dense groupings of buildings);
- b) The building line of new development should be in conformity with the existing. Very often, with terraced or dense groupings, the building line will be exactly the same, but in other cases it might be acceptable that it closely aligns with the exiting arrangement of buildings where there is an irregular, meandering building line;
- c) The density of any new infill development should reflect its context and its location in the village (centre or edge), or in a smaller settlement nestled in a wider landscape. The optimum density will respond to surrounding densities whilst making efficient use of land; and
- d) Where there are opportunities for infill development, proposals should demonstrate that existing views and vistas between buildings and along view corridors have been considered and the aim should be that they are retained, wherever possible. The sight lines, light and views between buildings are crucial to retaining character where infill development is proposed

A potential site for infill. The future infill property should complement the street scene.

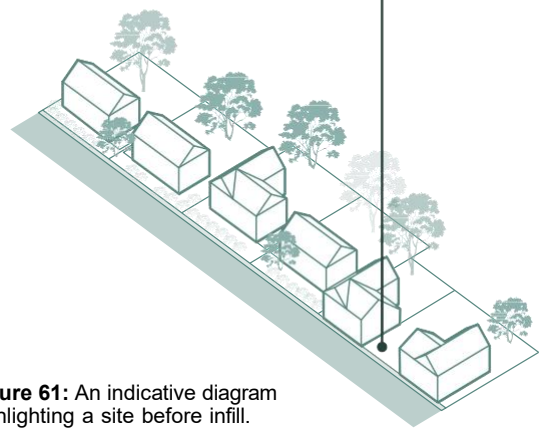


Figure 61: An indicative diagram highlighting a site before infill.

New building lines should be consistent with existing properties. Some places in the Parish have linear or regular meandering arrangements of buildings while others have random and irregular patterns. The infill should also reflect the surrounding context in terms of form, materials and height/massing.

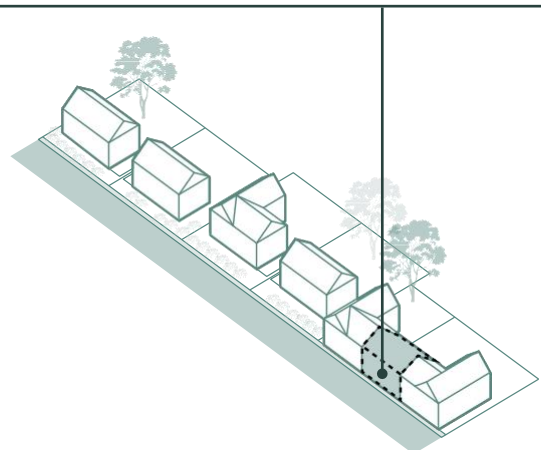


Figure 62: An indicative diagram highlighting a site after infill building.

BF 10 - ARCHITECTURE DETAILS, MATERIALS AND COLOUR PALETTE

Whilst much of the Parish’s housing stock was built in the 20th century, there are some earlier 19th, 18th and 17th century examples.

Some buildings have modern extensions and alterations. New developments should encourage and support innovative and proactive approaches to design and opportunities to deliver decentralised energy systems powered by a renewable or low carbon source and associated infrastructure, including community-led initiatives.

New developments should strive for good quality design that meets climatic targets for CO2 emissions and that can be constructed in a sustainable way, maximising opportunities for recycling.



Figure 63: Historic thatched cottage overlooking the River Test.



Figure 64: White render terraces on Winchester Street.

Informed by the local vernacular, the following pages illustrate acceptable materials and detailing for future housing developments in the Parish. The use of traditional construction finishes should be specified for all new development and repair work. Material specifications and quality should be upheld for repairs, replacements, and modern developments. The requirement for additional housing in the Parish should not trump architectural quality and character of the area.

Future developments should carefully apply this code to avoid creating a pastiche of the existing local vernacular. If traditional methods are used, they must be to a high quality that will allow the building to interact with the street at the same time as providing a good quality of life for the occupants.

Traditional building styles and detailing can be interpreted using contemporary methods to create a place that is both respectful to the existing character of Overton parish while having all the benefits of modern living. As well as this, high quality contemporary design is something that is likely to stand the test of time.



Figure 65: The Community Centre which uses flint and red brick.



Figure 66: Traditional porch in the village.

In the case of a conversion of an existing historic building into a residential use, this should look to preserve and enhance any existing heritage features, to maintain the integrity of the original building. Any new fenestration should be positioned carefully to maintain the character and balance of the building and reflect the existing design through use of complementary materials and finishes. These buildings create the opportunity to provide large single dwellings or can be split into a series of smaller dwellings.

Wall materials

There are different wall materials in the Parish such as red brick, white and cream render, flint, hung tiles and timber cladding .

Fenestration materials

There are various materials and styles used for windows and doors in the Parish such as sash, casement, dual aspect, wall dormer and bow windows, and apex pitched and flat porch roofs. Some windows have additional detailing. Poor examples of uPVC windows exist within the Parish and it is important for windows to be designed to function for views, passive heating and light.

While it is important to respect the character of the parish, it is also crucial that buildings are adaptable for future needs. For this reason the examples below show some contemporary window designs as well as traditional methods.

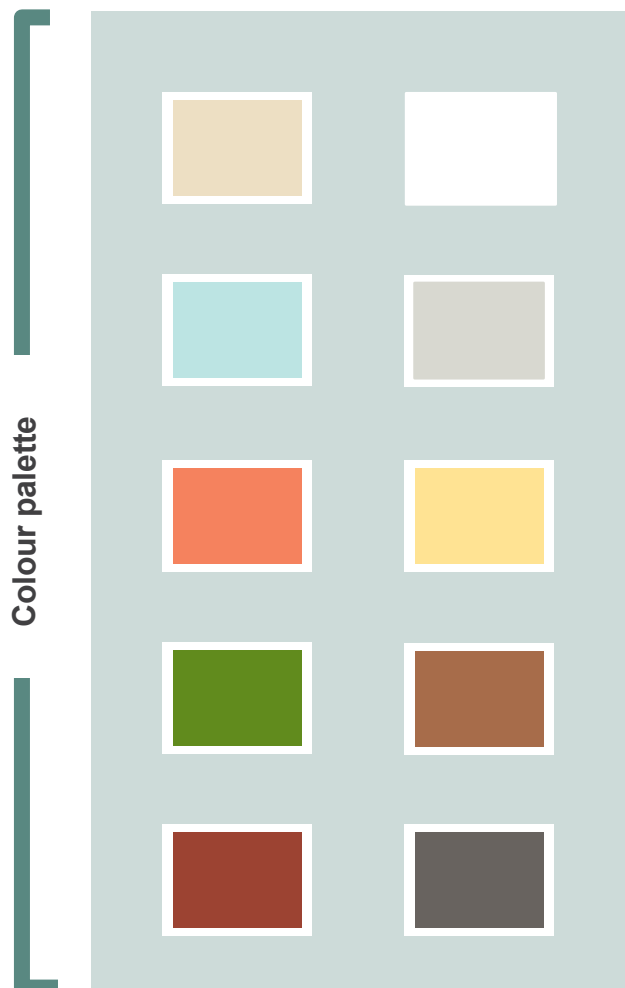
Roof materials

Of those roof materials in the Parish, red pantile and plain brown tile are more often used. The majority of buildings have pitched

roofs. PV arrays need to be considered as part of the roof arrangement and material choice. There are also some examples of slate and thatched houses in the older parts of the Parish.

Boundary treatment materials

There are a wide variety of boundary treatments in the village such as hedgerows, low walls with red and flint, soft landscaping and fencing.



Wall



Red brick



White/cream render



Flint



Painted render



Hung tiles

Penetration



Casement window



Bay window



Small storm window



Sash window



Roof dormer



Arched window



Pitched porch



Arched porch



Flat door canopy



Grey slates



Red pantiles



Thatched

Ground surface



Tarmac



Raised pedestrian crossing



Grass and other vegetation



Painted tarmac for cycle lanes



Gravel

EE. Environmental and energy efficiency

Design codes in the following section apply to the whole Parish. They contain important policies that will help to reduce our collective impact on the planet while allowing the natural environment in and around Overton Parish to flourish.

They include general guidance that apply to both new and existing development as some of the policies can be used to modify existing dwelling to become more environmentally sustainable.

Owing to the area's rich green space character, it is hoped that more of these policies are adopted in the future to help preserve and sustain this distinct character.

EE 01- FEATURES IN DWELLINGS

The following section elaborates on energy efficient technologies that could be incorporated in buildings and at broader Parish scale as principles.

Use of such principles and design tools should be encouraged in order to contribute towards a more sustainable environment.

Energy efficient or eco-design combines all around energy efficient appliances

and lighting with commercially available renewable energy systems, such as solar electricity and/or solar/ water heating and electric charging points.

All new dwellings should meet the Environmentally Sustainable Design standards set out in the Future Homes¹ Standard and the Basingstoke and Deane Local Plan².

1 <https://www.gov.uk/government/consultations/the-future-homes-and-buildings-standards-2023-consultation>
 2 <https://www.basingstoke.gov.uk/planningpolicy>

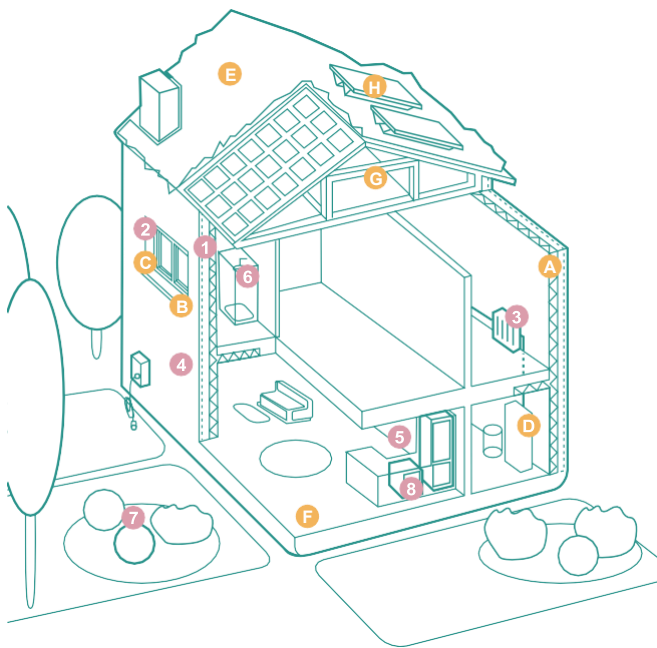













Figure 67: Diagram showing low-carbon homes in both existing and new build conditions.

Existing homes

- 1  **Insulation** in lofts and walls (cavity and solid)
- 2  **Double or triple glazing with shading** (e.g. tinted window film, blinds, curtains and trees outside)
- 3  **Low-carbon heating** with heat pumps or connections to district heat network
- 4  **Draught proofing** of floors, windows and doors
- 5  **Highly energy-efficient appliances** (e.g. A++ and A+++ rating)
- 6  **Highly waste-water efficient devices** with low-flow showers and taps, insulated tanks and hot water thermostats
- 7  **Green space (e.g. gardens and trees)** to help reduce the risks and impacts of flooding and overheating
- 8  **Flood resilience and resistance** with removable air back covers, relocated appliances (e.g. installing washing machines upstairs), treated wooden floors

Existing and new build homes

- A  **High levels of airtightness**
- B  **Triple glazed windows and external shading** especially on south and west faces
- C  **Low-carbon heating** and no new homes on the gas grid by 2025 at the latest
- D  **More fresh air** with mechanical ventilation and heat recovery, and passive cooling
- E  **Water management and cooling** more ambitious water efficiency standards, green roofs and reflective walls
- F  **Flood resilience and resistance** e.g. raised electrical, concrete floors and greening your garden
- G  **Construction and site planning** timber frames, sustainable transport options (such as cycling)
- H  **Solar panels**

EE 02- SUSTAINABLE URBAN DRAINAGE SYSTEM (SUDS)

SuDS cover a range of approaches to managing surface water in a more sustainable way to reduce flood risk and improve water quality whilst improving amenity benefits. Overton Parish wants SuDS to be integral to the design of Green Infrastructure to improve climate change resilience. Development should apply Building with Nature principles for SuDS.

SuDS work by reducing the amount and rate at which surface water reaches a waterway or combined sewer system. Usually, the most sustainable option is collecting this water for reuse, for example in a water butt or rainwater harvesting system.

Where reuse is not possible there are two alternative approaches using SuDS:

- a) The preferred option is infiltration, which allows water to percolate into the ground and eventually restore groundwater; and
- b) If infiltration is not feasible then attenuation and controlled release, which holds back the water and slowly releases it into the sewer network. Although the overall volume entering the sewer system is the same, the peak flow is reduced. This reduces the risk of sewers overflowing. Attenuation and controlled release options are suitable when either infiltration is not possible (for example where the water table is high or soils are clay) or where infiltration could be polluting (such as on contaminated sites).

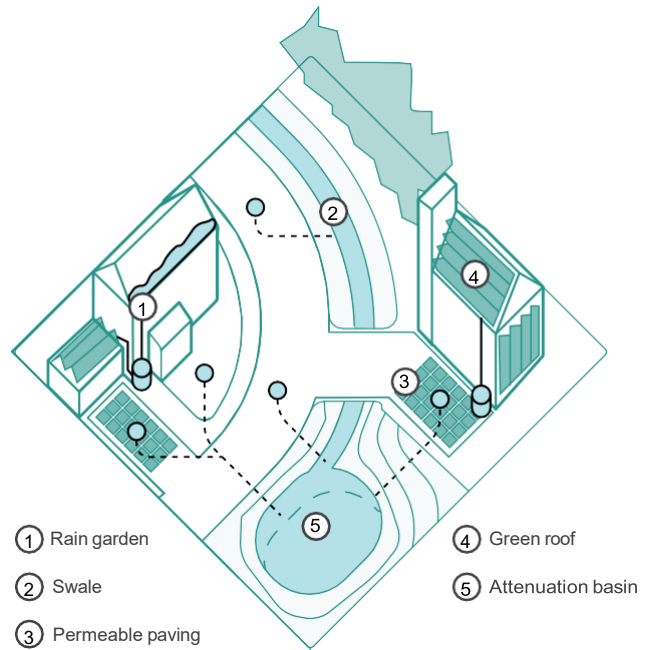


Figure 68: Diagram showing the best use of harvesting water systems rain garden, swales, permeable paving, green roofs

The most effective type or design of SuDS would depend on site-specific conditions such as underlying ground conditions, infiltration rate, slope, or presence of ground contamination. A number of overarching principles can however be applied:

- c) Reduce runoff rates by facilitating infiltration into the ground or by providing attenuation that stores water to help slow its flow down so that it does not overwhelm water courses or the sewer network;
- d) Integrate into development and improve amenity through early consideration in the development process and good design practices;
- e) SuDS are often as important in areas that are not directly in an area of flood risk themselves, as they can help reduce downstream flood risk by storing water upstream;
- f) Some of the most effective SuDS are vegetated, using natural processes to slow and clean the water whilst increasing the biodiversity value of the area;
- g) Tailing ponds and swales are good SuD options and should be located in appropriate locations;
- h) Best practice SuDS schemes link the water cycle to make the most efficient use of water resources by reusing surface water;

- i) SuDS must be designed sensitively to augment the landscape and provide biodiversity and amenity benefits; and
- j) Rain gardens where possible should be implemented onto streets of new developments as well as street trees. These will act as a form of urban drainage while softening the feel of the streetscape.



Figure 69: Examples of SuDS designed as a public amenity and fully integrated into the design of the public realm, Sweden.

EE 03- RAINWATER HARVESTING

Rainwater harvesting is a system for capturing and storing rainwater as well as enabling the reuse of in-situ grey water. It is important that new and existing developments follow the guidance below:

- a) Tanks should be concealed with complementary cladding;
- b) Use attractive materials or finishing for pipes, unsightly pipes should be avoided;
- c) Combine landscape or planters with water capture systems; and
- d) Underground tanks should be used where possible in Overton parish.



Figure 70: Example of a rainwater harvesting tank in the shape of a bee hive.



Figure 71: Example of a modular water tank.

EE 04- PERMEABLE PAVEMENTS

Most built-up areas, including roads and driveways, increase impervious surfaces and reduce the capacity of the ground to absorb runoff water. This in turn increases the risks of surface water flooding. Permeable pavements offer a solution to maintain soil permeability while performing the function of conventional paving. The choice of permeable paving units must be made depending on the local context; the units may take the form of unbound gravel, clay pavers, or stone setts. Suburban concrete pavers should be avoided.

Permeable paving can be used where appropriate on footpaths, public squares, private access roads, driveways, and private areas within the individual development boundaries.

It is recommended that the majority of the unbuilt areas in the plot (i.e. gardens) are permeable by means of landscape such as grass or earth as well as permeable and filtering pavements. As a rule of thumb the % of permeable area should be between 30% to 70%.

In addition, permeable pavement must also comply with:

- a) Flood and Water Management Act 2010, Schedule 3;¹
- b) The Building Regulations Part H – Drainage and Waste Disposal;²

¹ Great Britain (2010). Flood and Water Management Act, Schedule 3. Available at: <http://www.legislation.gov.uk/ukpga/2010/29/schedule/3>

² Great Britain (2010). The Building Regulations Part H – Drainage and Waste Disposal. Available at: https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/442889/BR_PDF_AD_H_2015.pdf

- c) Town and Country Planning (General Permitted Development) (England) Order 2015;³

Regulations, standards, and guidelines relevant to permeable paving and sustainable drainage are listed below:

³ Great Britain (2015). Town and Country Planning (General Permitted Development) (England) Order 2015.

Available at: http://www.legislation.gov.uk/ukxi/2015/596/pdfs/ukxi_20150596_en.pdf

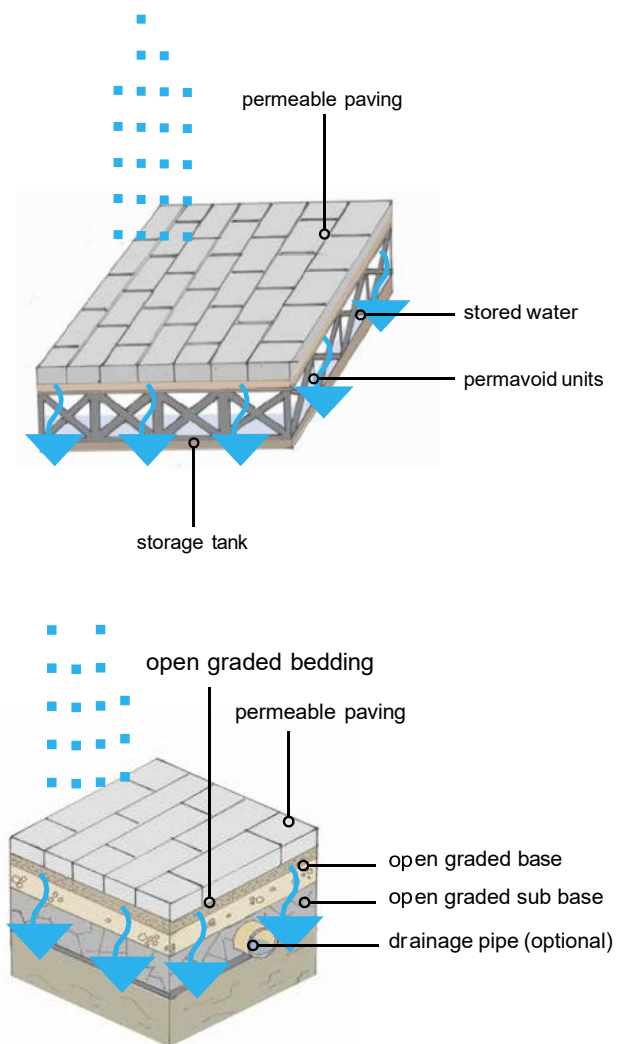


Figure 72: Diagrams illustrating the functioning of a soak away.

- d) Sustainable Drainage Systems - non- statutory technical standards for sustainable drainage systems;¹
- e) The SuDS Manual (C753);²
- f) BS 8582:2013 Code of practice for surface water management for development sites;³
- g) BS 7533-13:2009 Pavements constructed with clay, natural stone or concrete pavers;⁴ and
- h) Guidance on the Permeable Surfacing of Front Gardens.⁵

¹ Great Britain. Department for Environment, Food and Rural Affairs (2015). *Sustainable drainage systems – non-statutory technical standards for sustainable drainage systems*. Available at: https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/415773/sustainable-drainage-technical-standards.pdf

² CIRIA (2015). *The SuDS Manual (C753)*.

³ British Standards Institution (2013). *BS 8582:2013 Code of practice for surface water management for development sites*. Available at: <https://shop.bsigroup.com/ProductDetail/?pid=000000000030253266>

⁴ British Standards Institution (2009). *BS 7533-13:2009 Pavements constructed with clay, natural stone or concrete pavers*. Available at: <https://shop.bsigroup.com/ProductDetail/?pid=000000000030159352>

⁵ Great Britain. Ministry of Housing, Communities & Local Government (<https://www.gov.uk/government/publications/permeable-surfacing-of-front-gardens-guidance/guidance-on-the-permeable-surfacing-of-front-gardens>)



Figure 73: A good example of permeable paver (Source: <https://www.paverconnection.com/testimonial/hedwig-village-permeable-driveway-and-patio-upgrade/>)



Figure 74: A good example of clay paver (Source: <https://www.londonstone.co.uk/brick-pavers/>)

EE 05- WASTE STORAGE AND SERVICING

With modern requirements for waste separation and recycling, the number and size of household bins has increased. This poses a problem with the aesthetics of the property.

- a) Servicing arrangements should have a specific and attractive enclosure of sufficient size for all the necessary bins, this avoids the blocking of pavements with bins and makes the public realm more attractive. The storage solutions should be kept to the minimum dimensions in order to prevent the footprint being converted into an annexe at a later date;
- b) Create a specific enclosure of sufficient size for all the necessary bins;
- c) Bins should be placed as close to the dwelling's boundary and the public highway, such as against wall, fence or hedge;
- d) When coming forward with new proposals, developers should provide space for composting in their plans;
- e) Refer to the materials palette to analyse what would be a complementary material; and
- f) Create an environmentally sustainable enclosure to contain all bins.



Figure 75: Examples of successful storage design solutions for accommodating bins at the front of buildings.

EE 06- WILDLIFE FRIENDLY FEATURES

Biodiversity and woodlands should be protected and enhanced where possible.

- a) Roadside verges, hedges, and trees should act as natural buffers and should be protected when planning new developments;
- b) Abrupt edges to development with little vegetation or landscape on the edge
- c) of the settlement should be avoided and, instead, comprehensive landscape buffering should be encouraged;
- d) New developments and building extensions should aim to strengthen biodiversity and the natural environment;
- e) Ensure habitats are buffered. Widths of buffer zones should be wide enough and based on specific ecological function. These links should connect between existing green infrastructure to maintain or create new ecology corridors; and
- f) All fencing/walls to gardens should provide hedgehog holes.

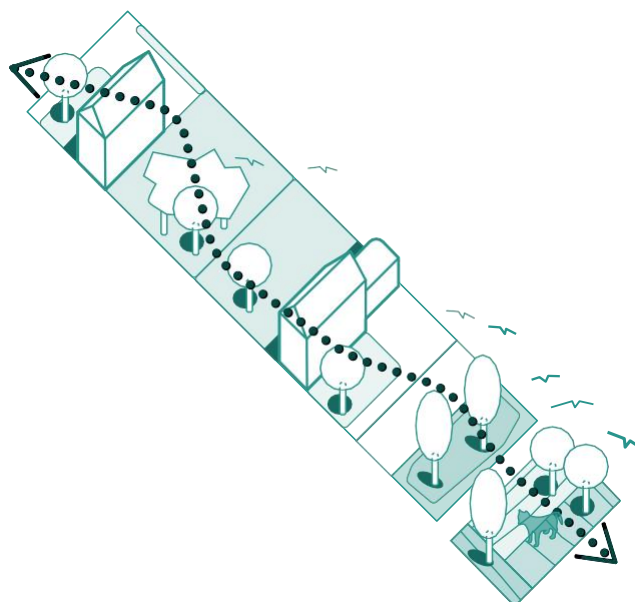
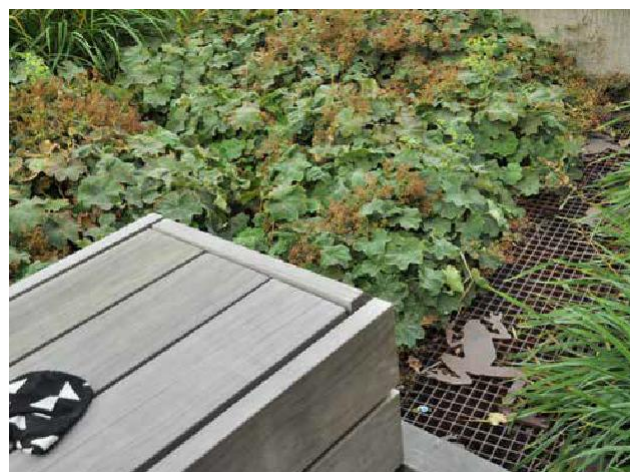


Figure 76: Diagram to highlight the importance of creating wildlife corridors (above).

Figure 77: Examples of a bughouse decorating rear gardens or public green spaces (left).

Figure 78: Examples of a frog habitat decorating rear gardens or public green spaces (right).



- g) New development proposals should include the creation of new habitats and wildlife corridors such as planting wildflowers and bulbs on the village green spaces, meadows and verges.
- h) This could be by aligning back and front gardens or installing nest boxes for birds or bricks in walls and improve habitat
- i) at ponds. Wildlife corridors should be included to enable local wildlife to travel to and from foraging areas and their dwelling area;
- j) Avoid low maintenance gardens which are harmful to wildlife by reducing hard landscaping; and
- k) The loss of any tree and garden should be discouraged. Encourage permeable pavement and gardens which is beneficial to biodiversity net gain.

EE 07 - ELECTRIC VEHICLE CHARGING POINTS

Overton Parish supports proposals for private transport using electric and other non-fossil fuel powered vehicles. These can be integrated both on-street and off-street. Here are some design guidelines for incorporating electric vehicle charging points in new developments:

On-street car parking or parking courts

- a) Car charging points should always be provided adjacent to public open spaces. Street trees and vegetation is also supported to minimise any visual contact with the charging points;
- b) Where charging points are located on the footpath, a clear footway width of
- c) 1.5m is required next to the charging point to avoid obstructing pedestrian flow; and
- d) Car charging points within parking courts are highly supported, since they can serve more than one vehicle.

Off-street car parking

- f) Mounted charging points and associated services should be integrated into the design of new developments, if possible with each house that provides off-street parking; and
- g) Cluttering elevations, especially main façades and front elevations, should be avoided.



Figure 79: Example of on-street electric vehicle charging points.



Figure 80: Example of electric vehicle charging points in a parking court.



Figure 81: Example of off-street electric vehicle charging points.

3.4 Checklist

As the design guidance and codes in this document cannot cover all design eventualities, this chapter provides a number of questions based on established good practice against which the design proposal should be evaluated. The aim is to assess all proposals by objectively answering the questions below. Not all the questions will apply to every development. The relevant ones, however, should provide an assessment as to whether the design proposal has considered the context and provided an adequate design solution.

As a first step there are a number of ideas or principles that should be present in all proposals. These are listed under 'General design guidance for new development'. Following these ideas and principles, questions are listed for more specific topics on the following pages.

1

General design guidelines for new development:

- New development will integrate with existing paths, streets, circulation networks and patterns of activity;
- Reinforce or enhance the established settlement character of streets, greens, and other spaces;
- Harmonise with and enhance the existing settlement in terms of physical form, architecture and land use;
- Relate well to local topography and landscape features, including prominent vegetation and long-distance views;
- Reflect, respect, and reinforce local architecture and historic distinctiveness;
- Retain and incorporate important existing features into the development;
- Respect surrounding buildings in terms of scale, height, form and massing;
- Adopt contextually appropriate materials and details;
- Provide adequate open space for the development in terms of both quantity and quality;
- Incorporate necessary services and drainage infrastructure without causing unacceptable harm to retained features;
- Ensure all components e.g. buildings, landscapes, access routes, parking and open space are well related to each other;
- Make sufficient provision for sustainable waste management (including facilities for kerbside collection, waste separation, and minimisation where appropriate) without adverse impact on the street scene, the local landscape or the amenities of neighbours;
- Positively integrate energy efficient technologies;
- Ensure that places are designed with management, maintenance and the upkeep of utilities in mind; and
- Seek to implement passive environmental design principles by, firstly, considering how the site layout can optimise beneficial solar gain and reduce energy demands (e.g. insulation), before specification of energy efficient building services and finally incorporate renewable energy sources.

2

Local green spaces, views & character:

- What are the particular characteristics of this area which have been taken into account in the design; i.e. what are the landscape qualities of the area?
- Does the proposal maintain or enhance any identified views or views in general?
- How does the proposal affect the trees on or adjacent to the site?
- Can trees be used to provide natural shading from unwanted solar gain? i.e. deciduous trees can limit solar gains in summer, while maximising them in winter.
- Has the proposal been considered within its wider physical context?
- Has the impact on the landscape quality of the area been taken into account?
- In rural locations, has the impact of the development on the tranquillity of the area been fully considered?
- How does the proposal impact on existing views which are important to the area and how are these views incorporated in the design?
- Can any new views be created?
- Is there adequate amenity space for the development?
- Does the new development respect and enhance existing amenity space?
- Have opportunities for enhancing existing amenity spaces been explored?
- Will any communal amenity space be created? If so, how this will be used by the new owners and how will it be managed?
- Is there opportunity to increase the local area biodiversity?
- Can green space be used for natural flood prevention e.g. permeable landscaping, swales etc.?
- Can water bodies be used to provide evaporative cooling?
- Is there space to consider a ground source heat pump array, either horizontal ground loop or borehole (if excavation is required)?
- Does the development provide a minimum of 10% BNG, communal green space and integrated green/blue infrastructure?

3

Street grid and layout:

- Does it favour accessibility and connectivity? If not, why?
- Do the new points of access and street layout have regard for all users of the development; in particular pedestrians, cyclists and those with disabilities?
- What are the essential characteristics of the existing street pattern; are these reflected in the proposal?
- How will the new design or extension integrate with the existing street arrangement?
- Are the new points of access appropriate in terms of patterns of movement?
- Do the points of access conform to the statutory technical requirements?

4

Gateway and access features:

- What is the arrival point, how is it designed?
- Does the proposal maintain or enhance the existing gaps between settlements?
- Does the proposal affect or change the setting of a listed building or listed landscape?
- Is the landscaping to be hard or soft?

5

Buildings layout and grouping:

- What are the typical groupings of buildings?
- How have the existing groupings been reflected in the proposal?
- Are proposed groups of buildings offering variety and texture to the townscape?
- What effect would the proposal have on the streetscape?
- Does the proposal maintain the character of dwelling clusters stemming from the main road?
- Does the proposal overlook any adjacent properties or gardens? How is this mitigated?
- Subject to topography and the clustering of existing buildings, are new buildings oriented to incorporate passive solar design principles, with, for example, one of the main glazed elevations within 30° due south, whilst also minimising overheating risk?
- Can buildings with complementary energy profiles be clustered together such that a communal low carbon energy source could be used to supply multiple buildings that might require energy at different times of day or night? This is to reduce peak loads. And/or can waste heat from one building be extracted to provide cooling to that building as well as heat to another building?

6

Building line and boundary treatment:

- What are the characteristics of the building line?
- How has the building line been respected in the proposals?
- Has the appropriateness of the boundary treatments been considered in the context of the site?

7

Building heights and roofline:

- What are the characteristics of the roofline?
- Have the proposals paid careful attention to height, form, massing and scale?
- If a higher than average building(s) is proposed, what would be the reason for making the development higher?
- Will the roof structure be capable of supporting a photovoltaic or solar thermal array?
- Will the inclusion of roof mounted renewable technologies be an issue from a visual or planning perspective? If so, can they be screened from view, being careful not to cause over shading?

8

Household extensions:

- Does the proposed design respect the character of the area and the immediate neighbourhood?
- What is the impact of the proposed changes/extension on the surrounding environment, including green space and parking/pedestrian access?
- Is the roof form of the extension appropriate to the original dwelling?
- Do the proposed materials match those of the existing dwelling?
- In case of side extensions, does it retain important gaps within the street scene and avoid a 'terracing effect'?
- Are there any proposed dormer roof extensions set within the roof slope?
- Does the proposed extension respond to the existing pattern of window and door openings?
- Is the side extension set back from the front of the house?
- Does the extension offer the opportunity to retrofit energy efficiency measures to the existing building?
- Can any materials be re-used in situ to reduce waste and embodied carbon?
- What is the impact of the proposed changes/extension on the surrounding environment, including green space and parking/pedestrian access?

9

Building materials & surface treatment:

- What is the distinctive material in the area?
- Does the proposed material harmonise with the local materials?
- Does the proposal use high-quality materials?
- Have the details of the windows, doors, eaves and roof details been addressed in the context of the overall design?
- Do the new proposed materials respect or enhance the existing area or adversely change its character?
- Are recycled materials, or those with high recycled content proposed?
- Has the embodied carbon of the materials been considered and are there options which can reduce the embodied carbon of the design? For example, wood structures and concrete alternatives.
- Can the proposed materials be locally and/or responsibly sourced? E.g. FSC timber, or certified under BES 6001, ISO 14001 Environmental Management Systems?

10

Car parking:

- What parking solutions have been considered?
- Are the car spaces located and arranged in a way that is not dominant or detrimental to the sense of place?
- Has planting been considered to soften the presence of cars?
- Does the proposed car parking compromise the amenity of adjoining properties?
- Have the needs of wheelchair users been considered?
- Can electric vehicle charging points be provided?
- Can secure cycle storage be provided at an individual building level or through a central/ communal facility where appropriate?
- If covered car ports or cycle storage is included, can it incorporate roof mounted photovoltaic panels or a biodiverse roof in its design?
- Has adequate off road parking been provided for each dwelling?
- Does the proposed parking arrangement provide sufficient security and deter anti-social behaviour/crime?

11

Architectural details and design:

- Does the proposal harmonise with the adjacent properties? This means that it follows the height massing and general proportions of adjacent buildings and how it takes cues from materials and other physical characteristics.
- Does the proposal maintain or enhance the existing landscape features?
- Has the local architectural character and precedent been demonstrated in the proposals?
- If the proposal is a contemporary design, are the details and materials of a sufficiently high enough quality and does it relate specifically to the architectural characteristics and scale of the site?
- Is it possible to incorporate passive environmental design features such as larger roof overhangs, deeper window reveals and/or external louvres/shutters to provide shading in hotter months?
- Can the building designs utilise thermal mass to minimise heat transfer and provide free cooling?

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